





10 PROPOSED DESIGN COMMERCIAL

CHOCOLATE FACTORY &
BLOCK I (MET POLICE SITE)



10.2 THE CHOCOLATE FACTORY- REFURBISHMENT



Initial concept sketches, views of internal courtyard and proposed circulation

Lightweight roof structure added above the internal courtyard to create an attractive gallery/ exhibition space/ central meeting zone and visually connecting studios

Additional floor

Renovated elevation maintaining the original character and detail

Welcoming entrance

Rearrangement of internal layout with studios accessed from corridor located around internal courtyard. Access and communication to be focused around the 'hub' to give sense of connection and identity.

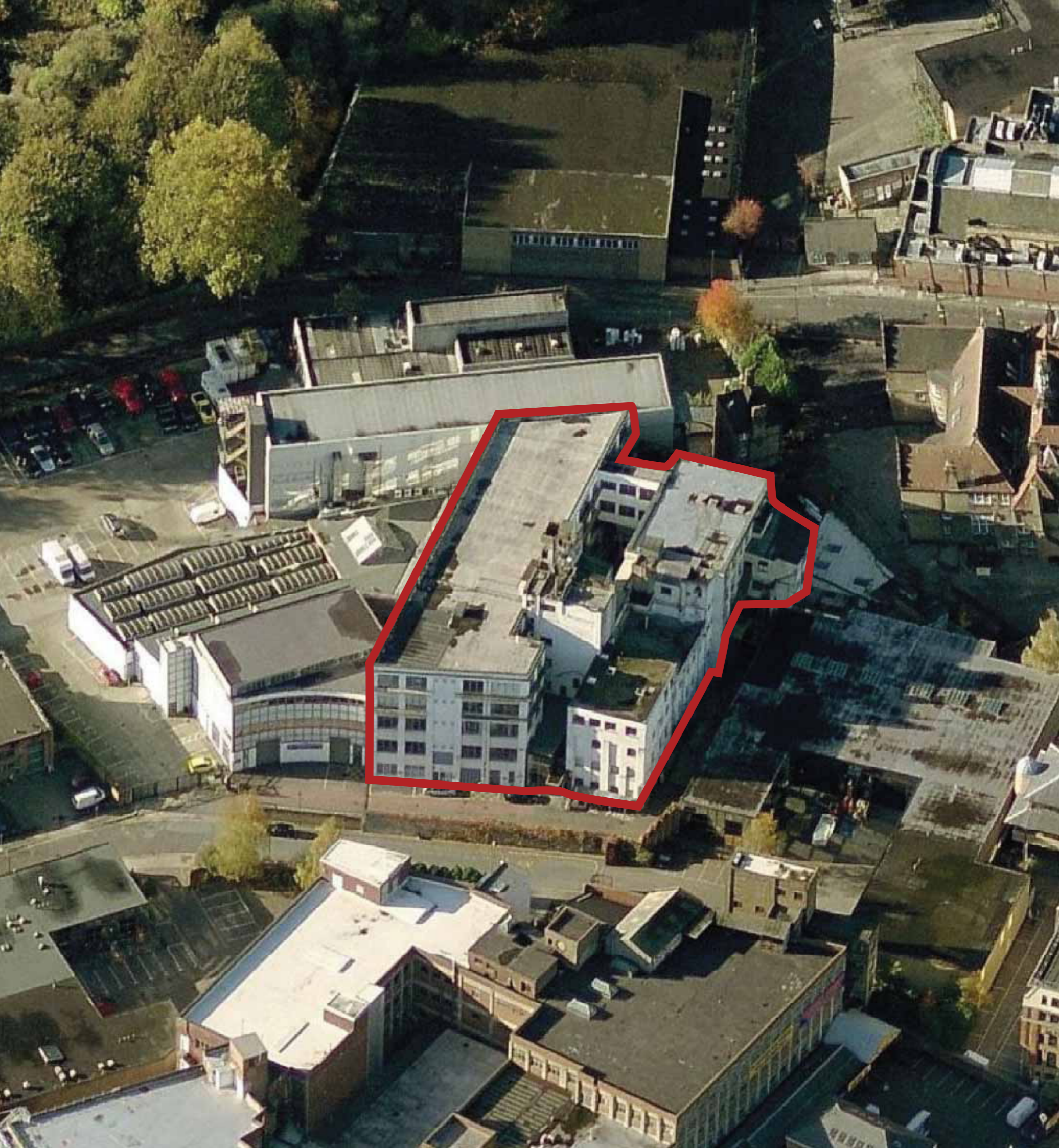
Rearrangement of internal layout introducing new stair core and lifts location as well as revised location of the sanitary blocks for better movement and accessibility

Active frontage/ bar/ cafe

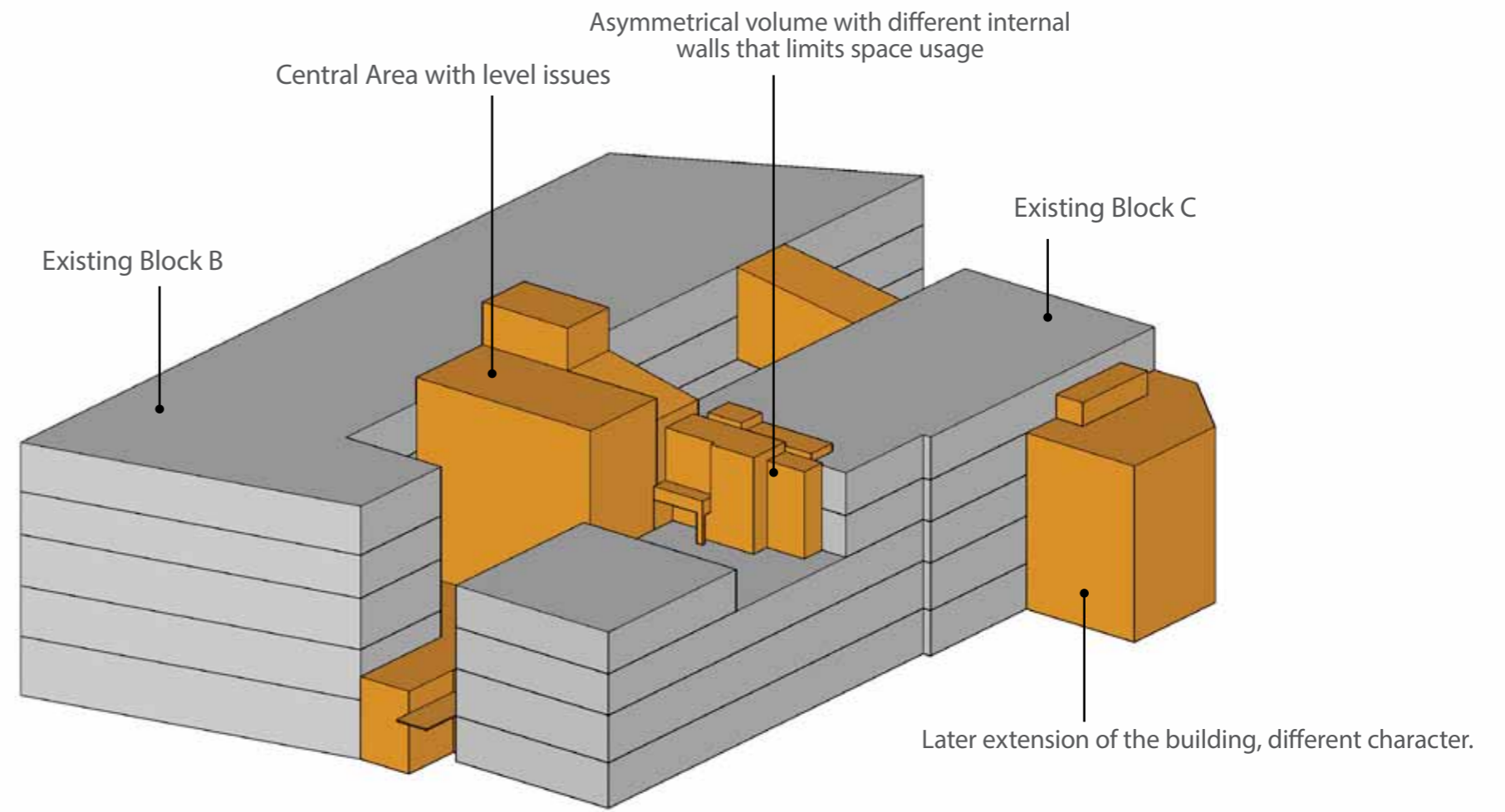
Covered courtyard/ exhibition space,/ hub



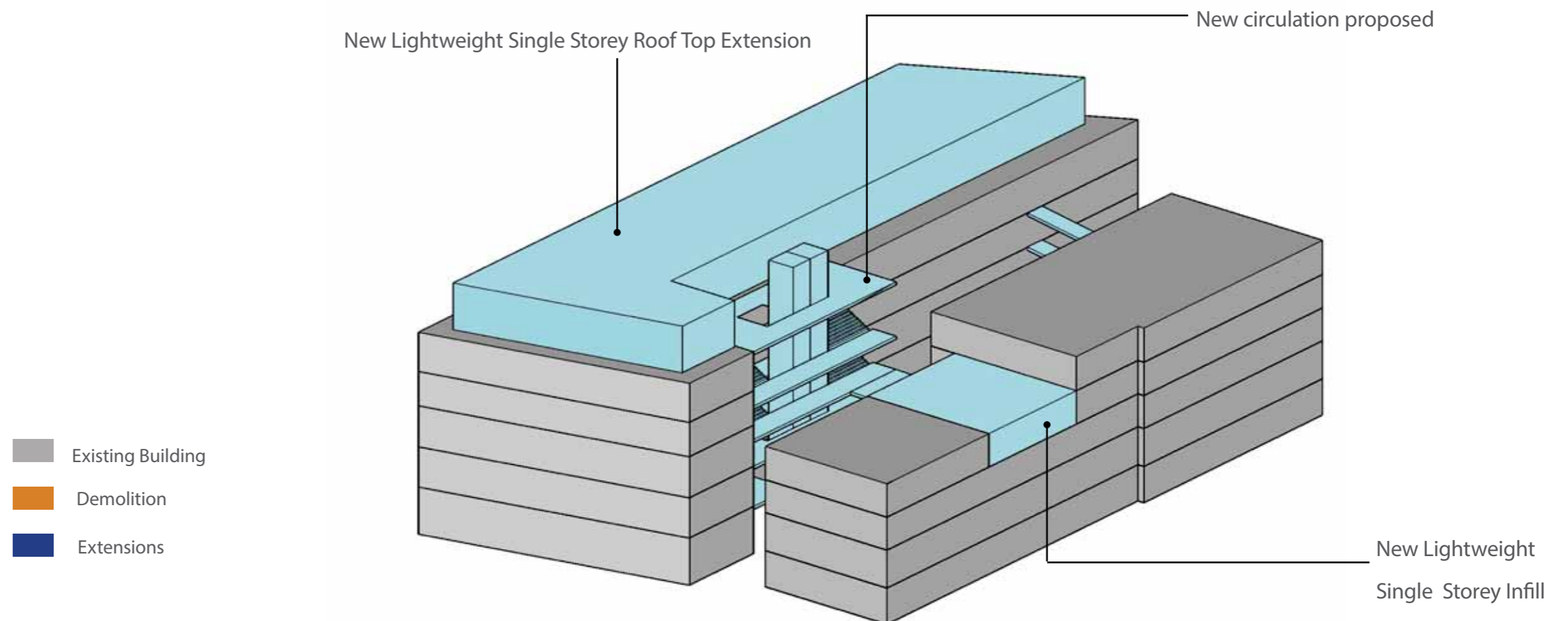
Aerial photo- existing Chocolate
Factory



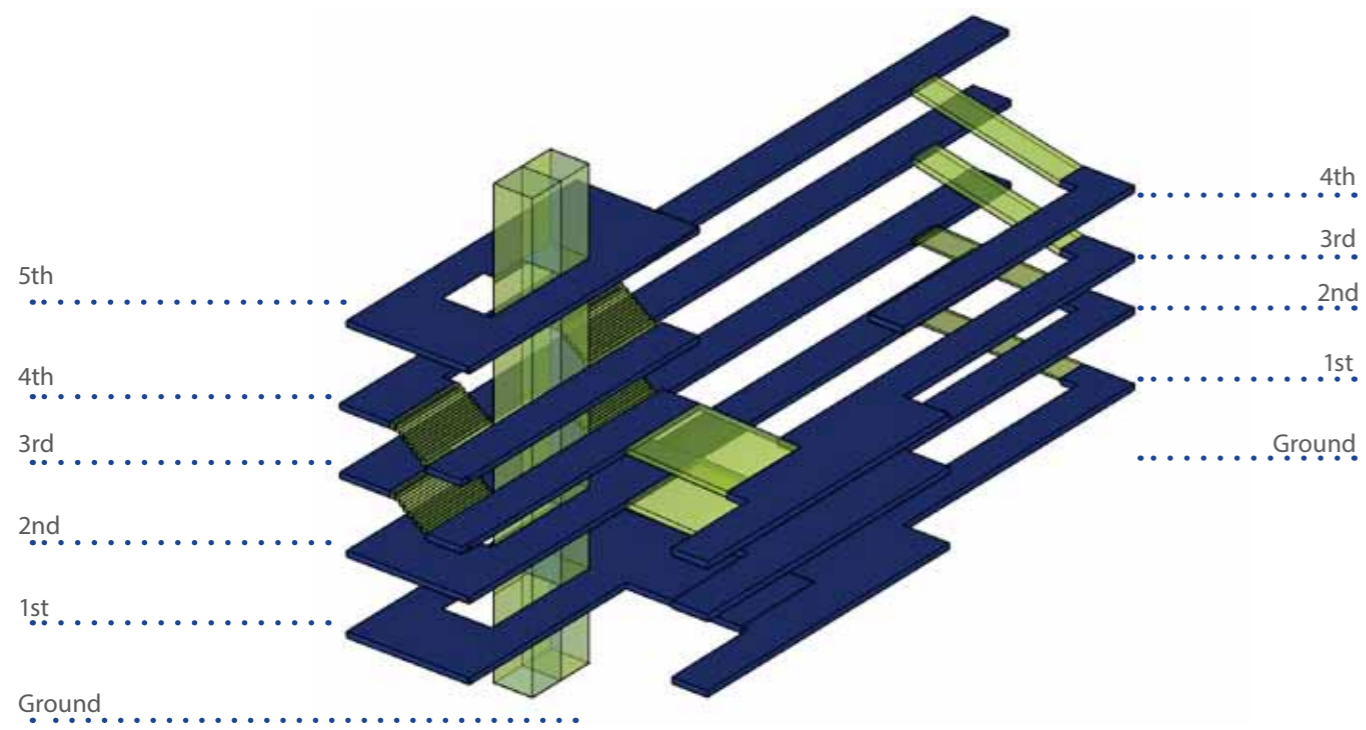
Extend of Demolition Proposed



Extend of Extensions Proposed



Circulation Diagram



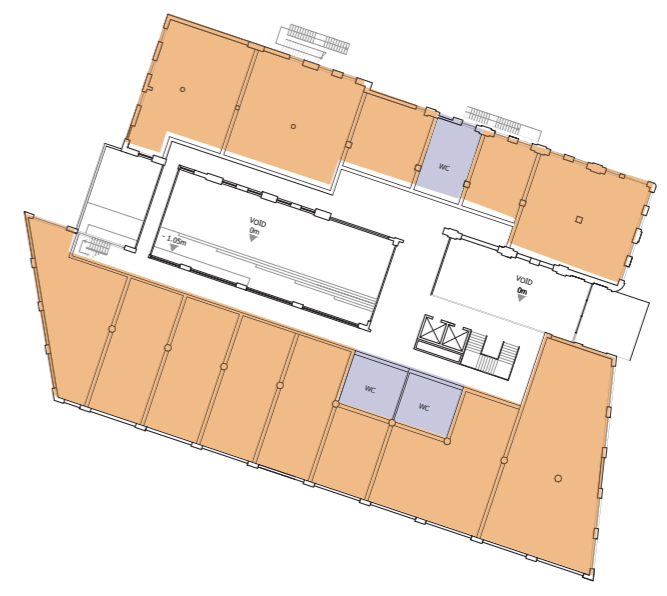
Use Diagram



Ground Floor



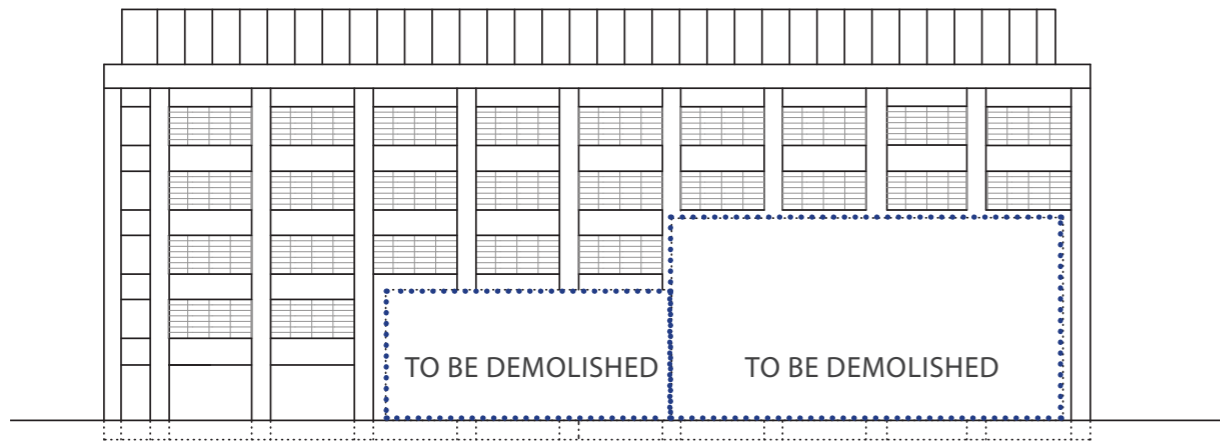
Typical Floor



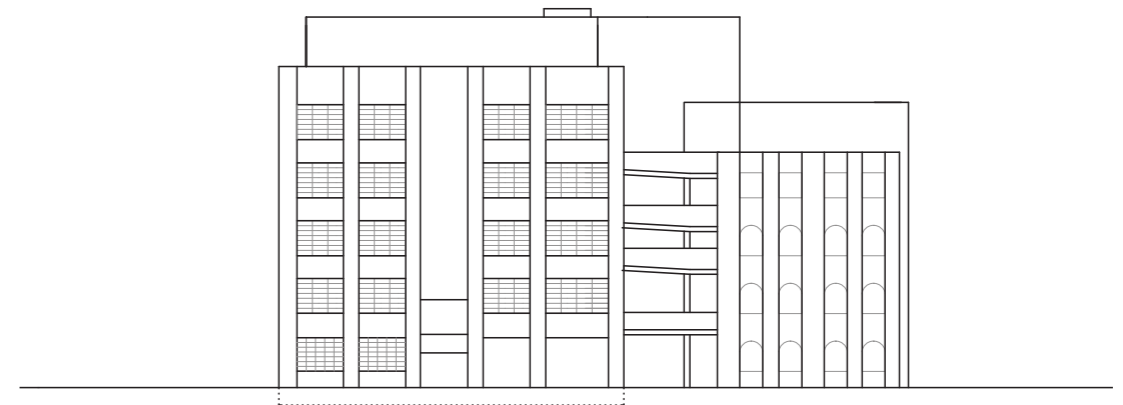
Typical Floor

- Horizontal Movement
- Vertical Movement

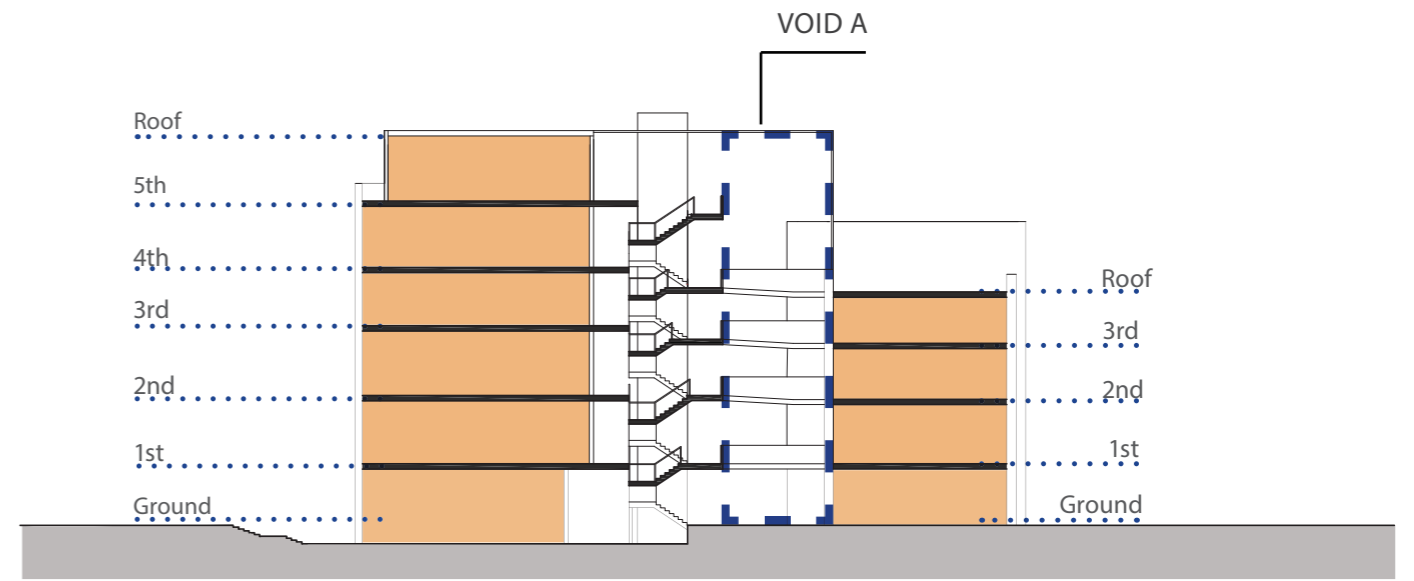
- Commercial
- Cafe
- Potential Exhibition Space
- WC
- Principal Access



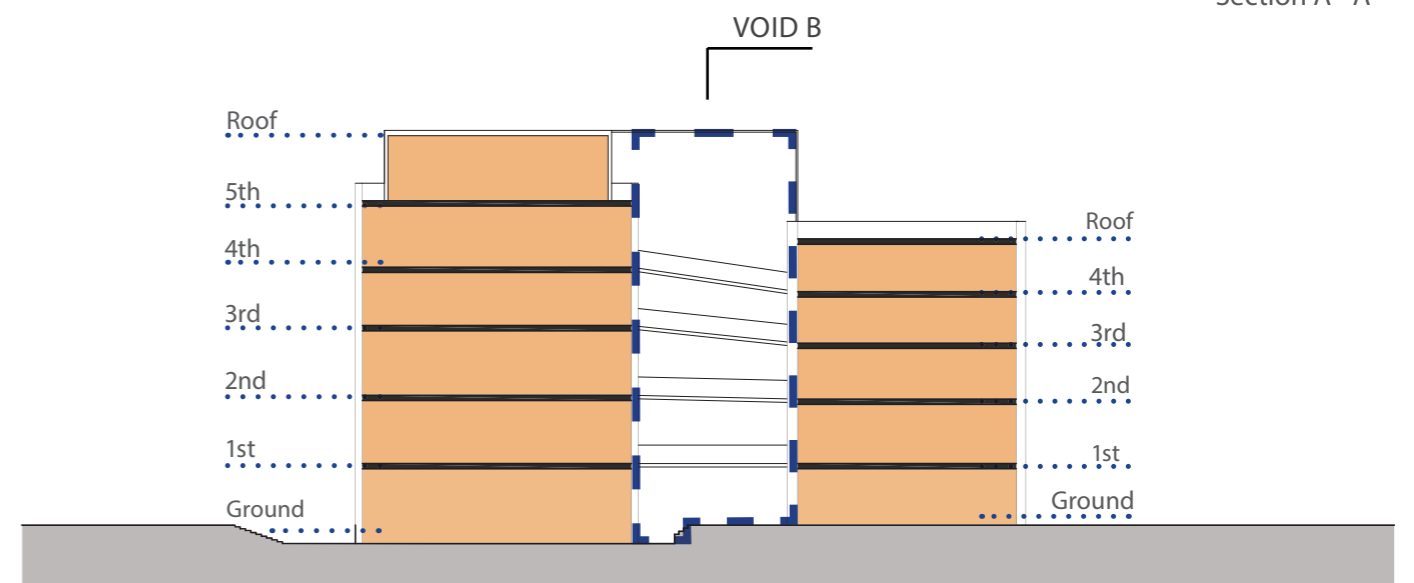
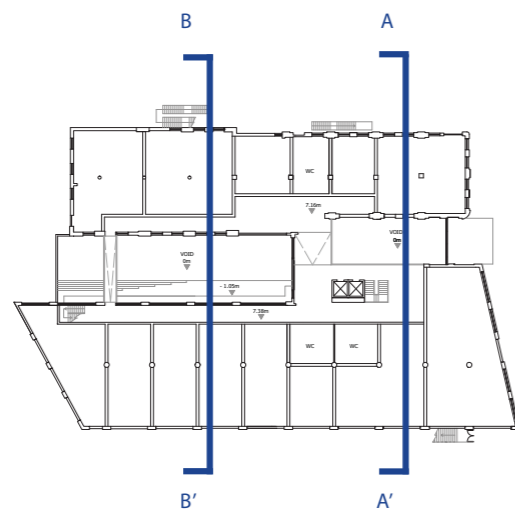
South Elevation



Front Elevation

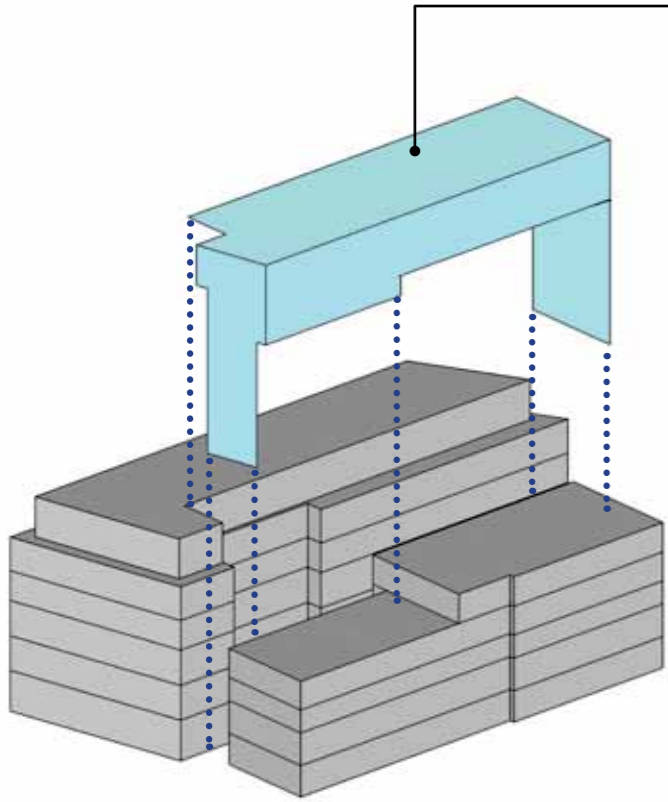


Section A - A'

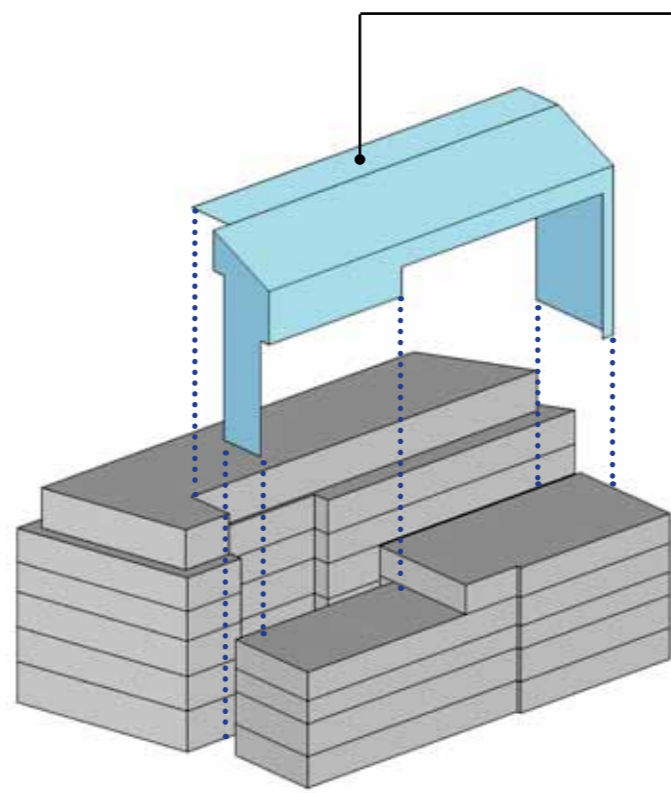


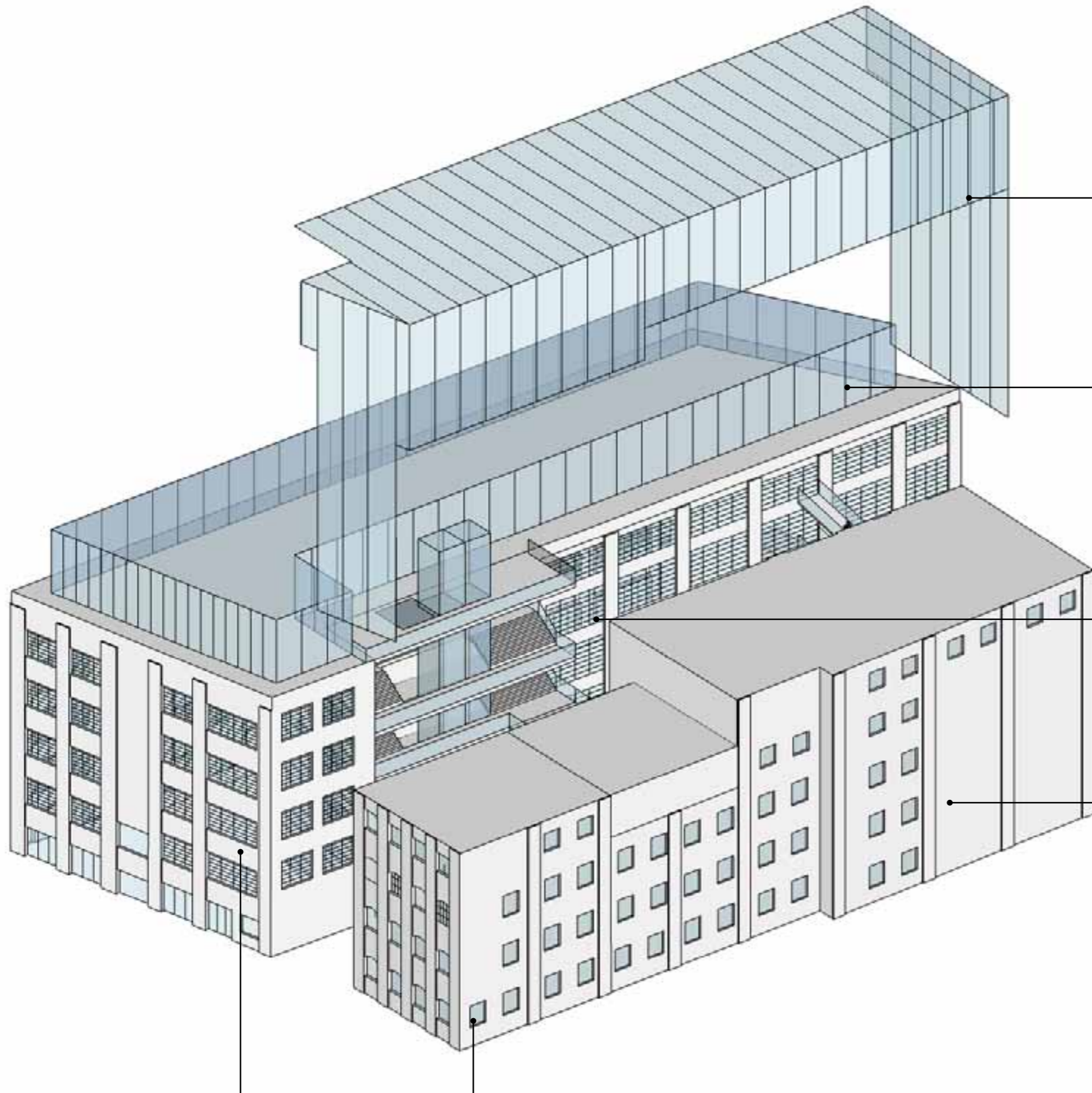
Section B - B'

Flat Roof

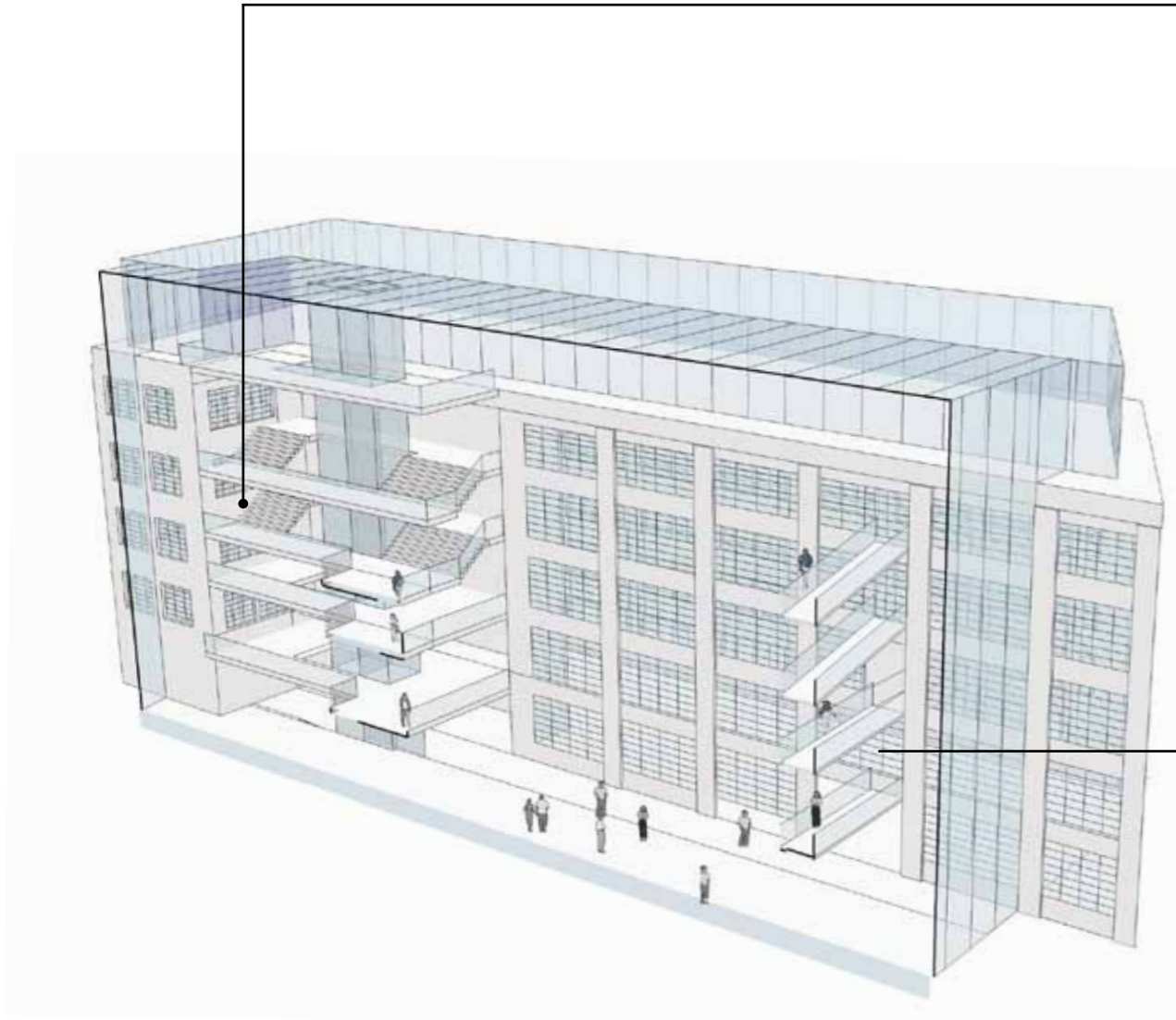


Chamfered Roof





View from internal courtyard towards the main entrance.



Section C-C, 3d view

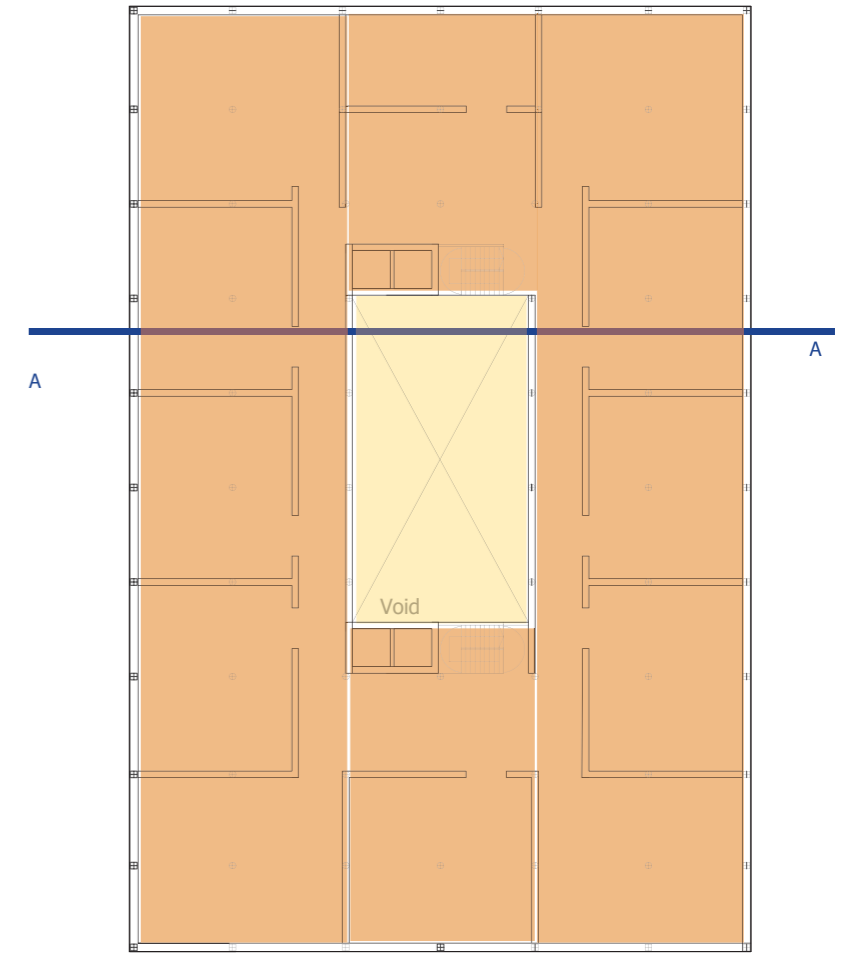
View from internal courtyard looking west



10.3 BLOCK I (METROPOLITAN POLICE SITE)

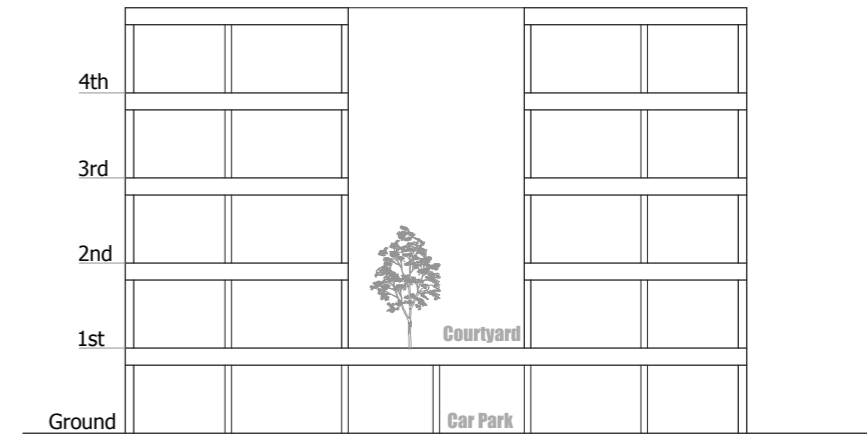


Ground Floor



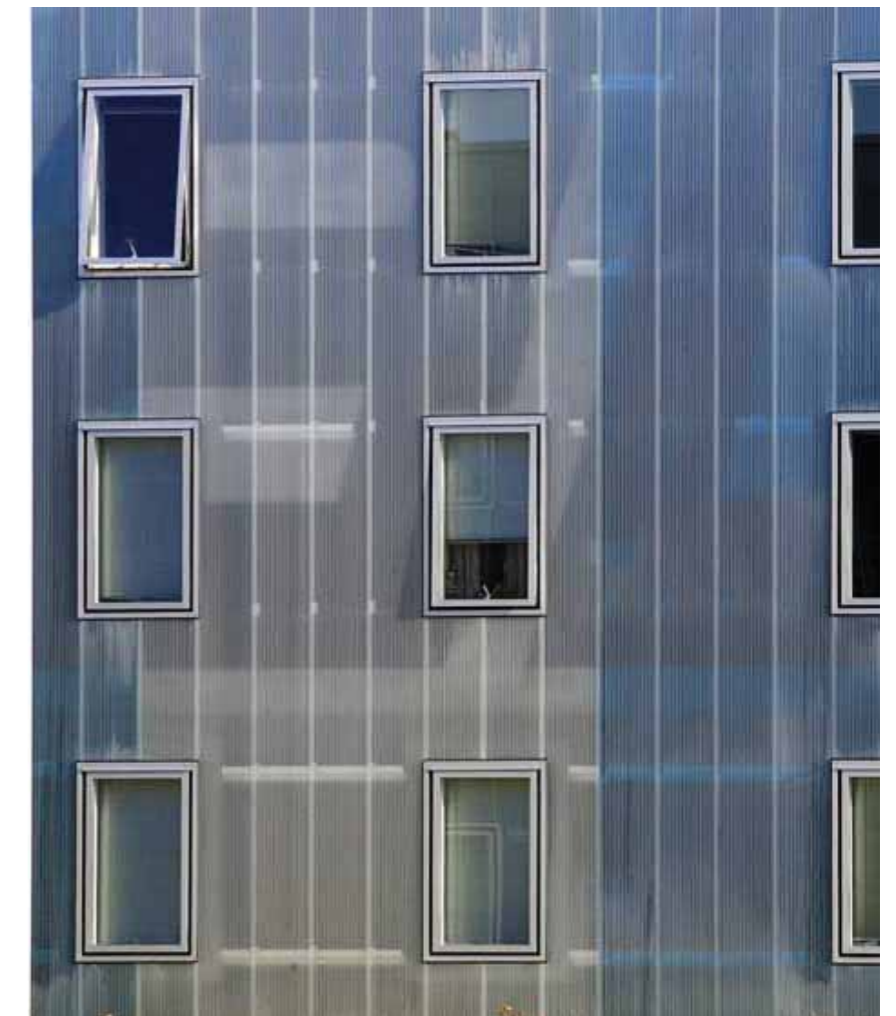
Typical Floor (1st to 4th floor)

- Commercial
- Reception/ Entrance lobby
- Communal amenity space- at first floor only
- Principal Access



Section A-A

Precedent images of elevation treatment





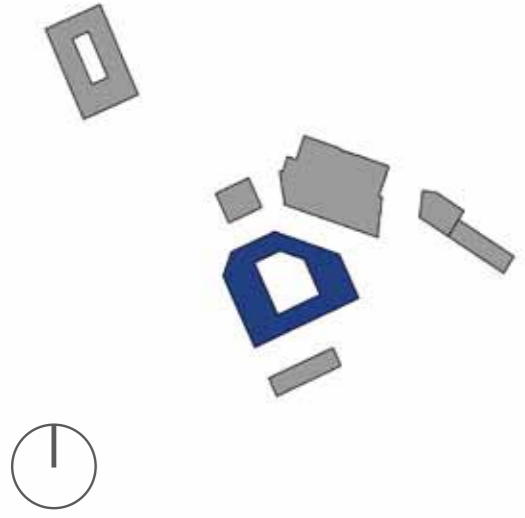
11 PROPOSED DESIGN RESIDENTIAL/MIXED USE



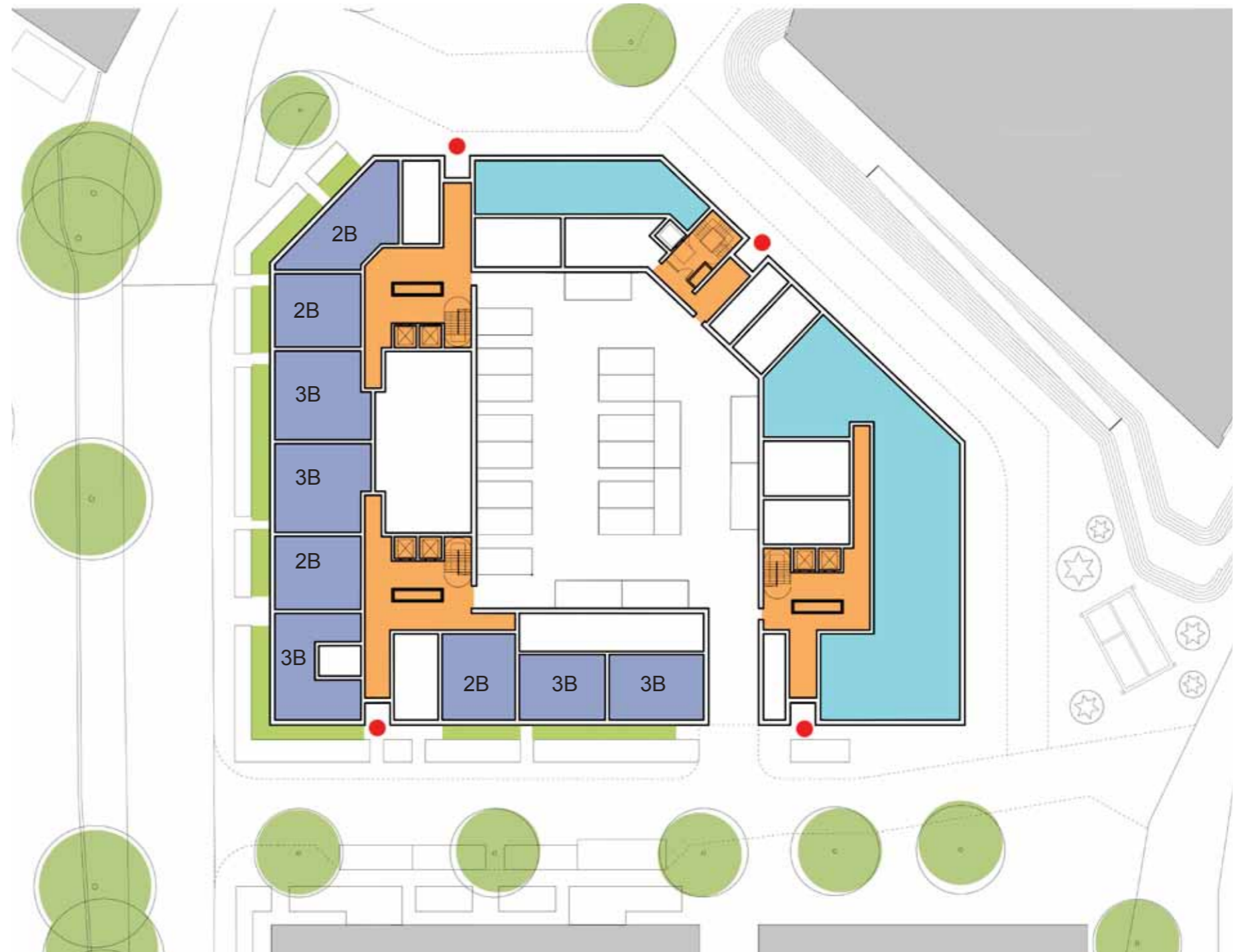


11.2 DETAILED PLANNING APPLICATION- RESIDENTIAL- BLOCK LAYOUTS

BLOCK E

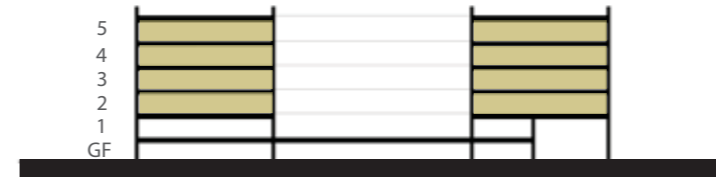


- Circulations Cores
- Commercial
- Duplex Units
- Flat Units
- Amenity Spaces
- Access
- Bin/ Bike Store/ Plant

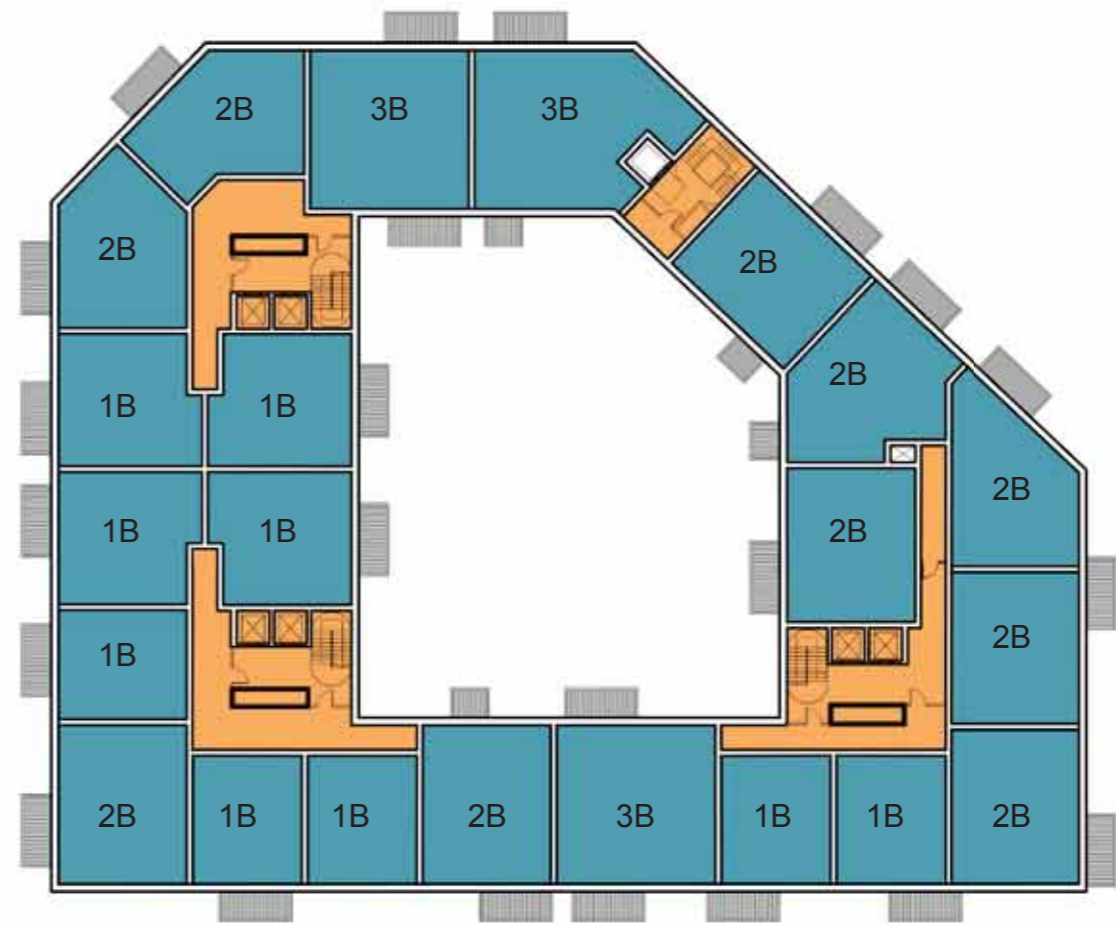


Ground Floor Plan

BLOCK E

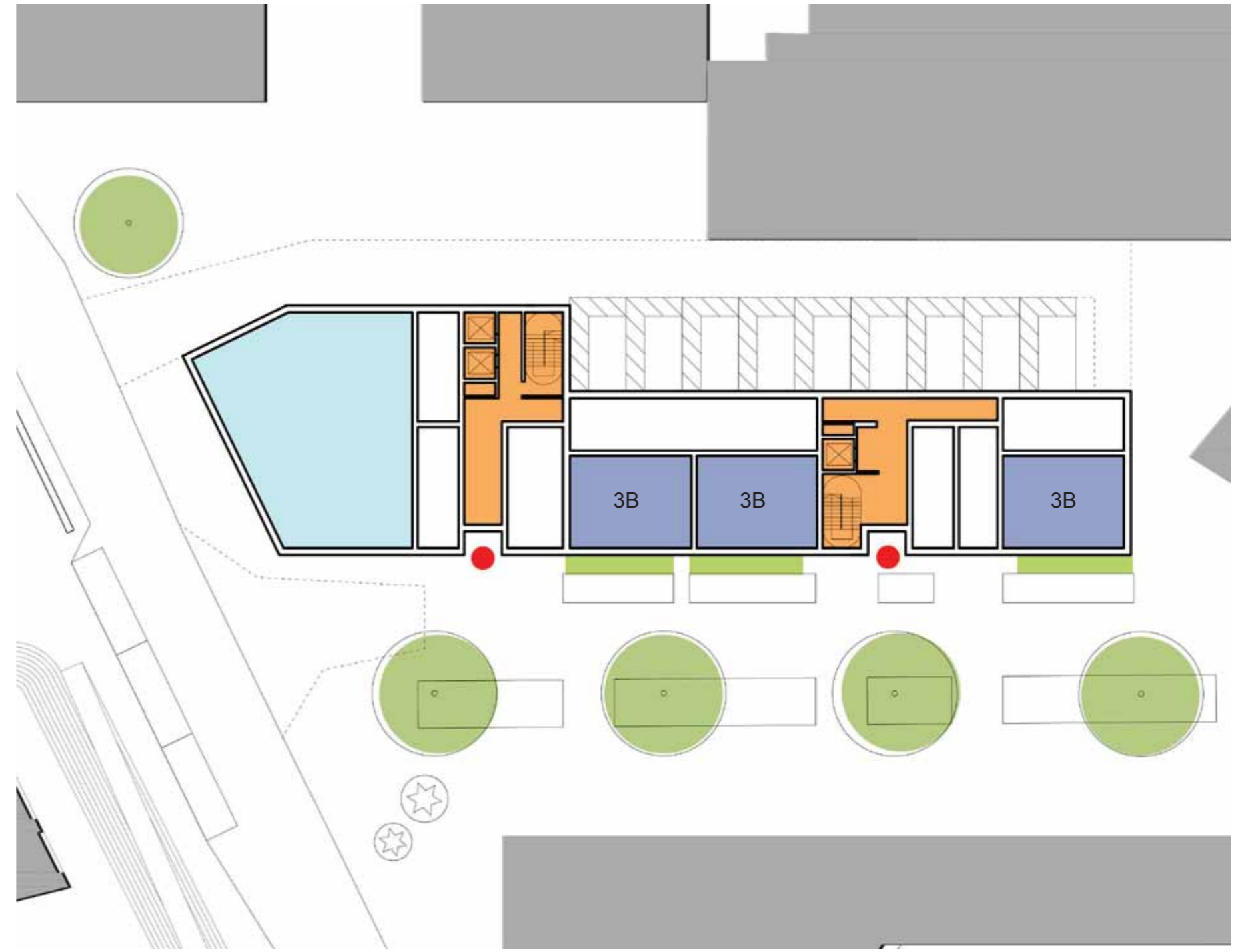
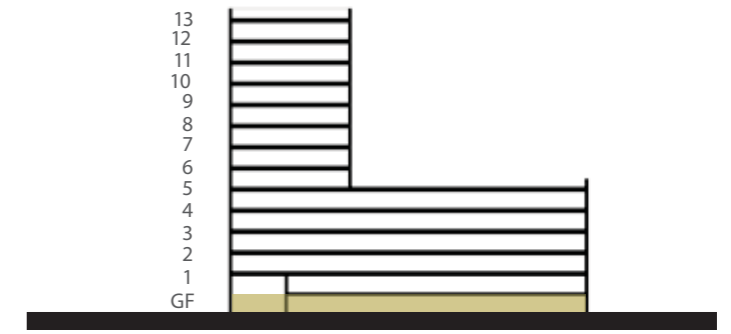
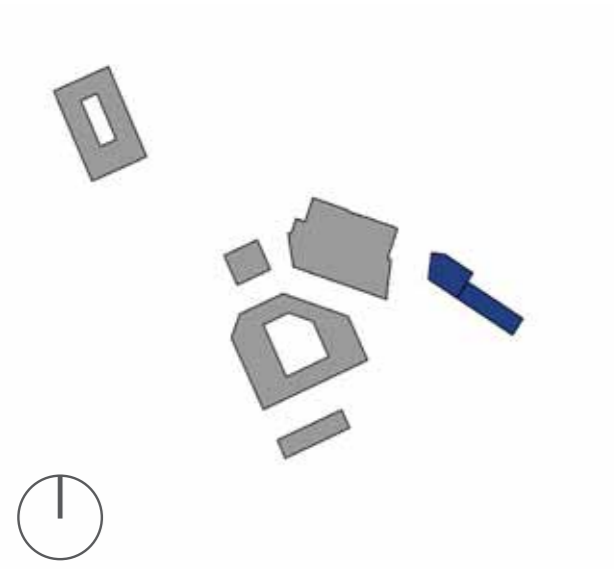


First Floor Plan



Typical Floor Plan

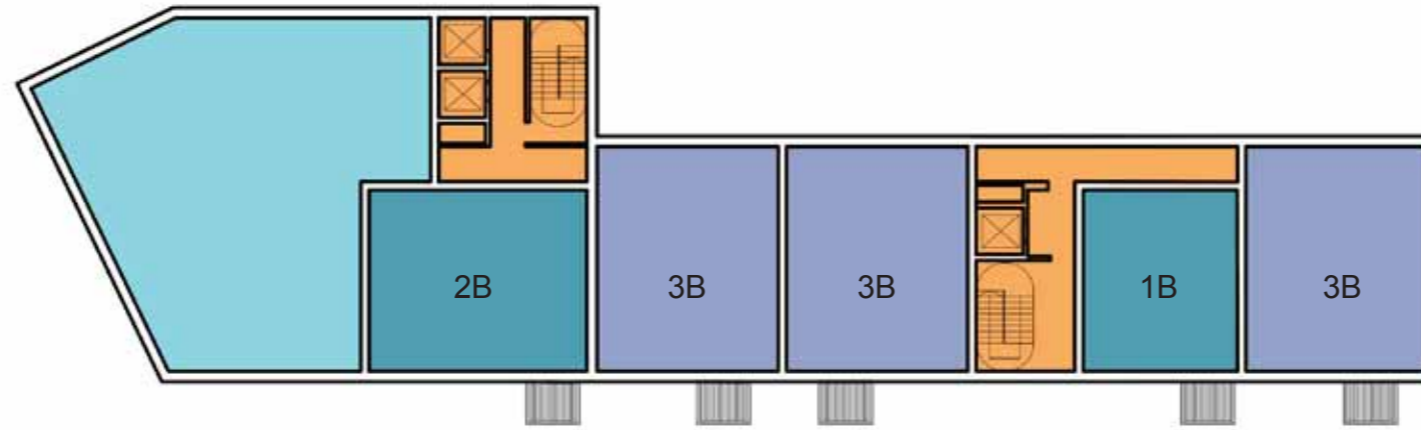
BLOCK B



- Circulations Cores
- Commercial
- Duplex Units
- Flat Units
- Amenity Spaces
- Access
- Bin/ Bike Store/ Plant

Ground Floor Plan

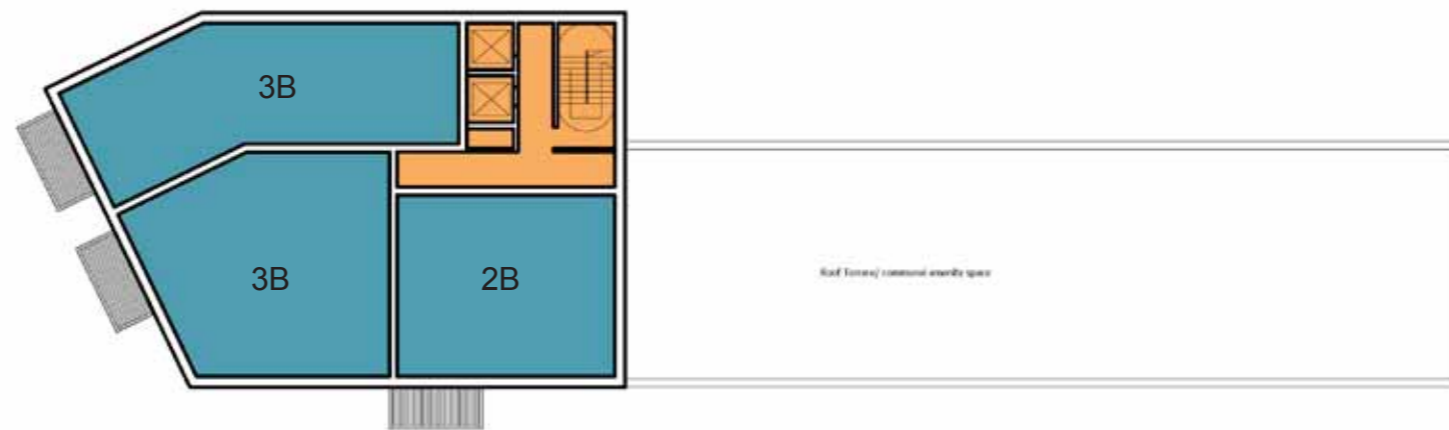
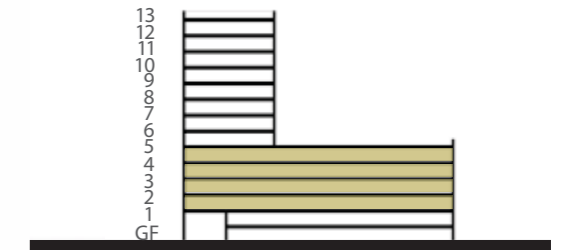
BLOCK B



First Floor Plan



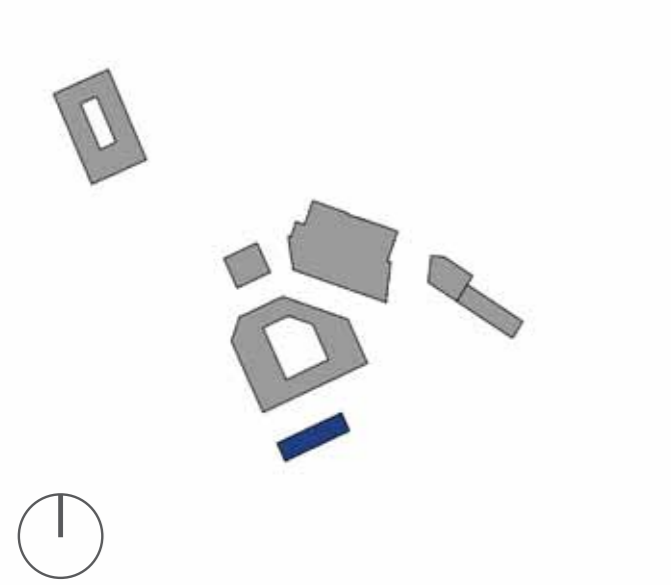
Typical Floor Plan (2nd-5th)



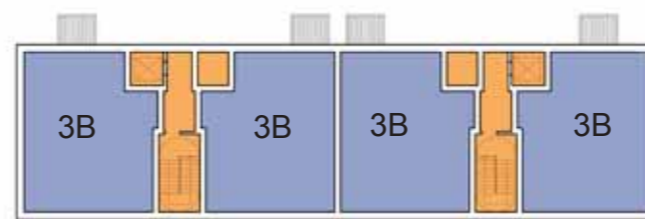
Typical Floor Plan (6th-13th)



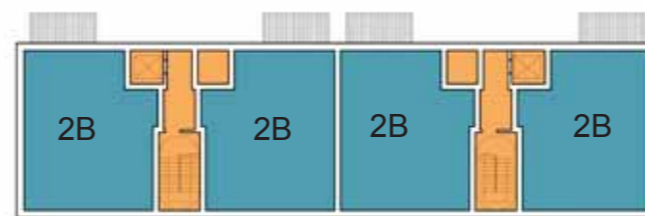
BLOCK D1



Ground Floor Plan

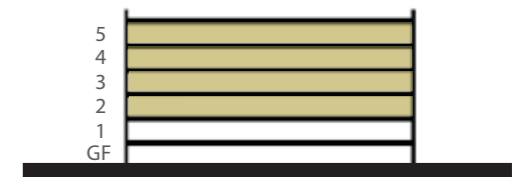
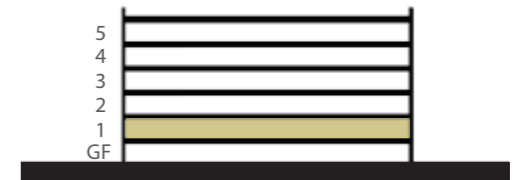


First Floor Plan

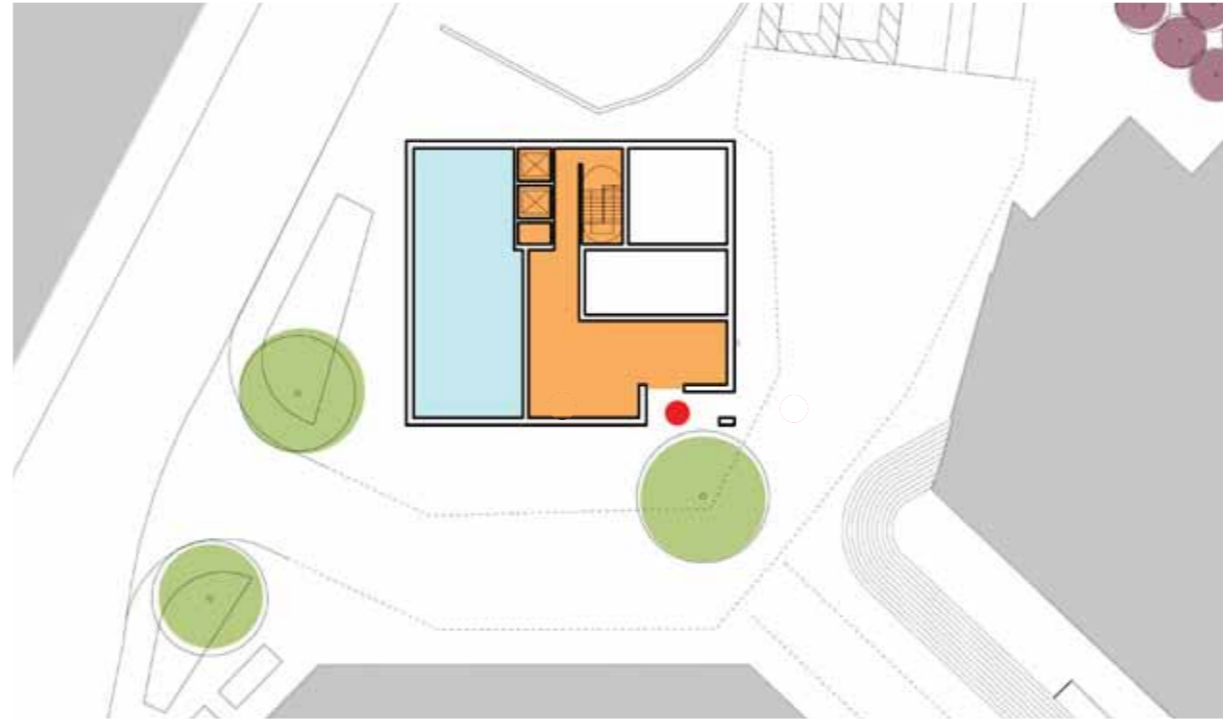
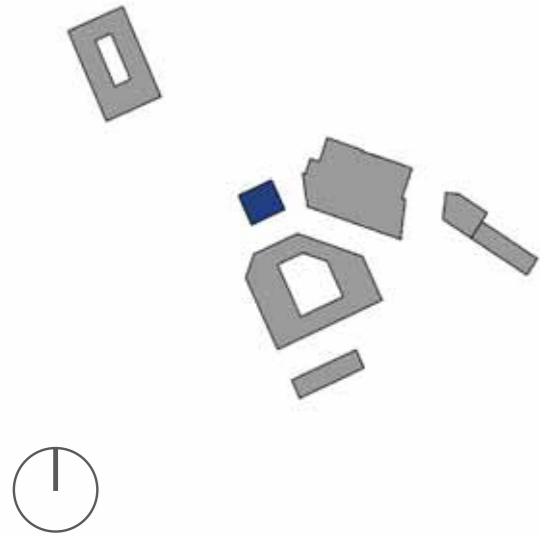


Typical Floor Plan

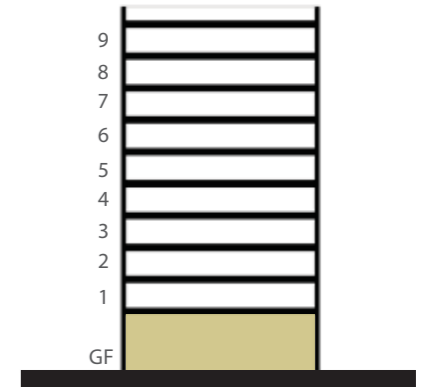
- Circulations Cores
- Commercial
- Duplex Units
- Flat Units
- Amenity Spaces
- Access
- Bin/ Bike Store/ Plant



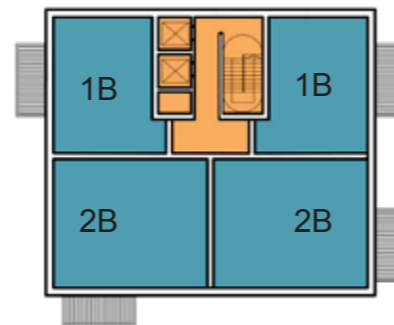
BLOCK F



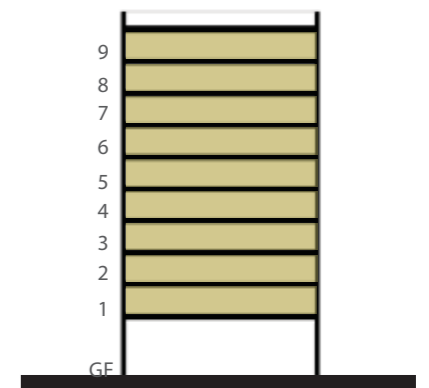
Ground Floor Plan



- Circulations Cores
- Commercial
- Flat Units
- Access
- Bin/ Bike Store/ Plant



Typical Floor Plan

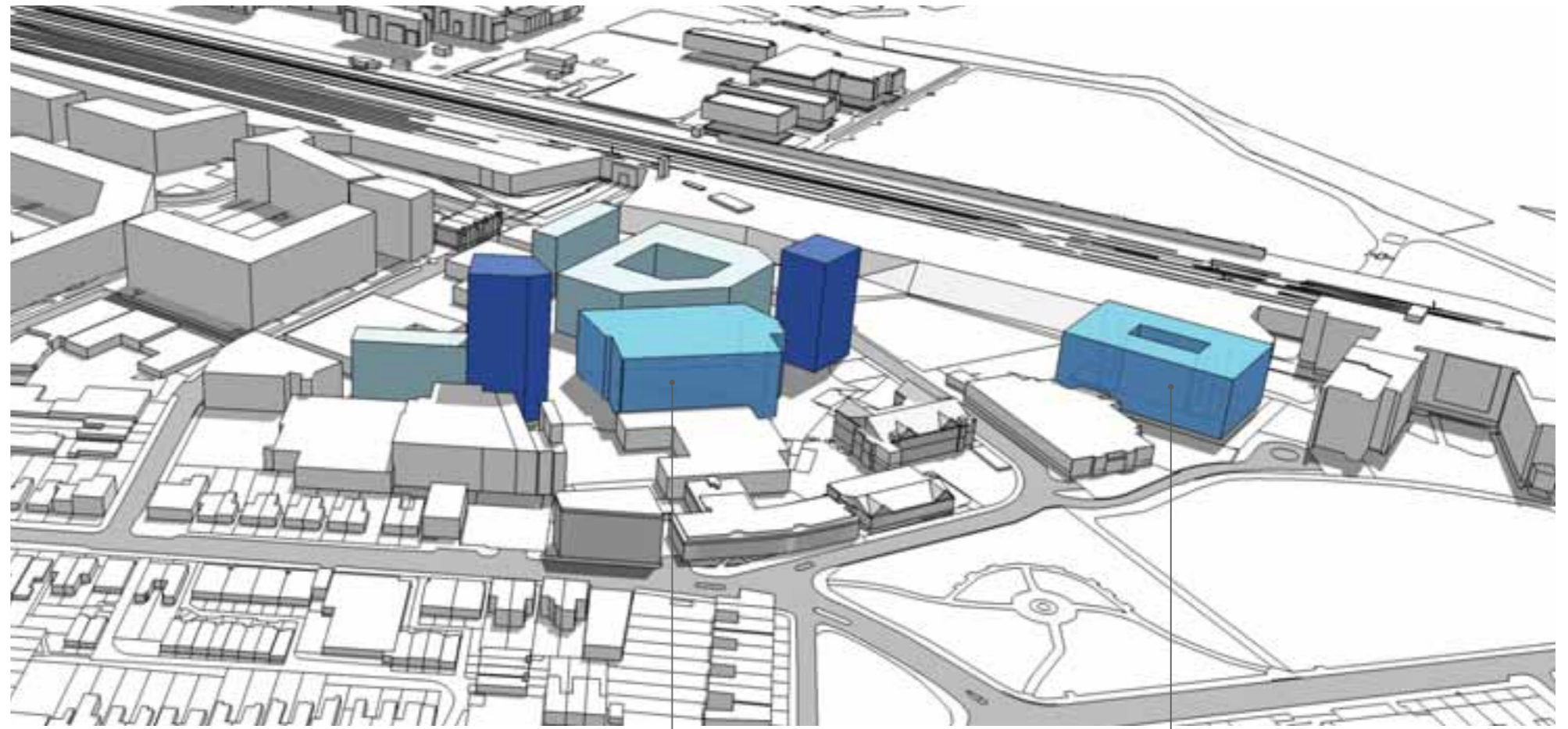


11.3 ELEVATION STUDIES - DESIGN APPROACH

While defining the design approach a particular attention has been given to the proportions of the massing.

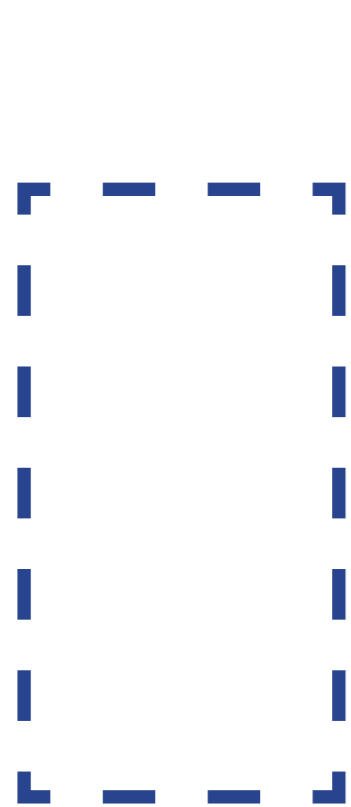
Two massing types have been identified across the site:

- ■ Horizontal
(Buildings up to 6 storeys)
- ■ Vertical
(Buildings above 6 storeys)



The Chocolate Factory
(Commercial building refurbishment)

Block I
Former site of Met Police
(Commercial building)



Block F



Block B (Tower)



Block B



Block E

Block D1

11.4 ELEVATION STUDIES - PATTERN

OPTION 1

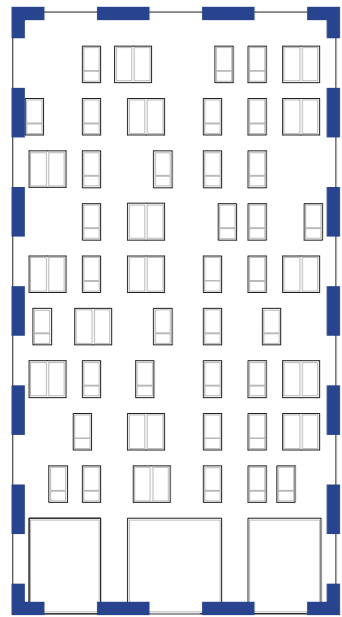
WALL WITH INFORMAL PATTERN



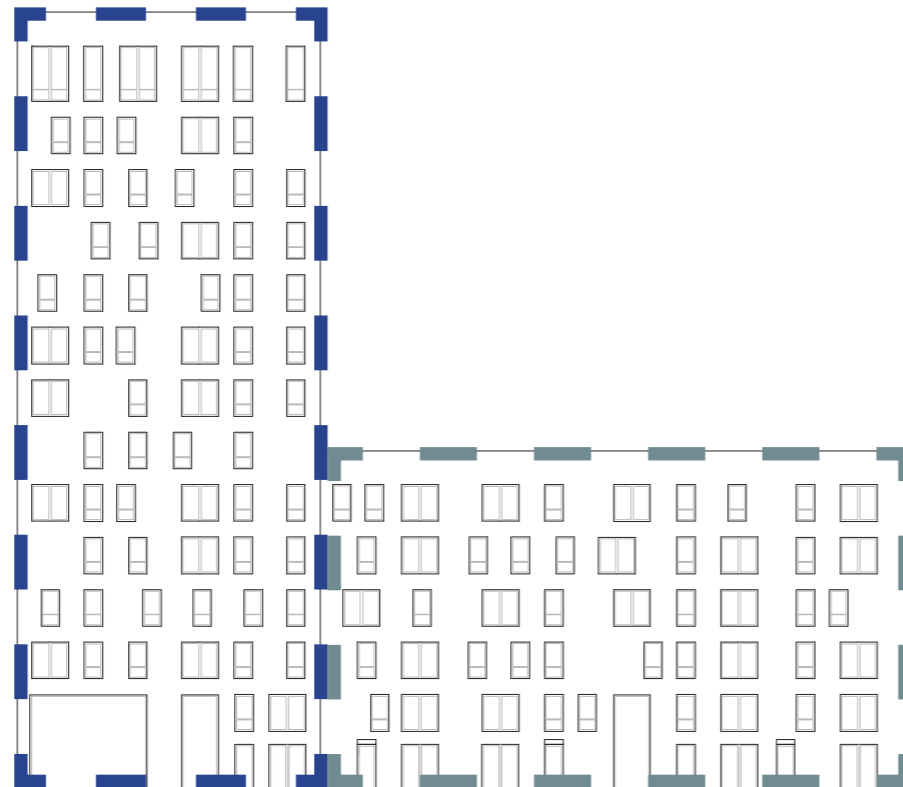
Block E (horizontal massing)

Block B Tower (vertical massing)

View from Western Road looking East at the shared zone between Block E and D (corner of Block E)



Block F



Block B (Tower)

Block B



Block E

Block D1

OPTION 2

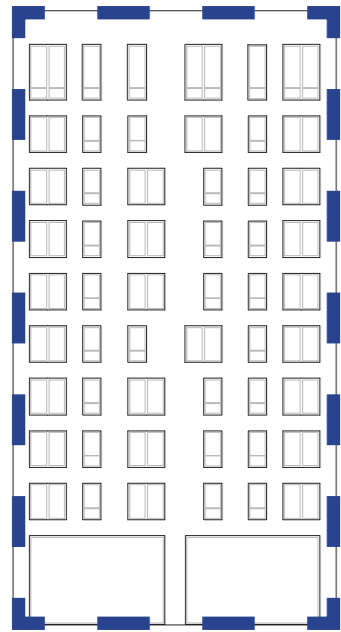
WALL WITH FORMAL PATTERN WITH VARIANT TO TALLER ELEMENTS (WINDOW MOVEMENT) - PREFERRED



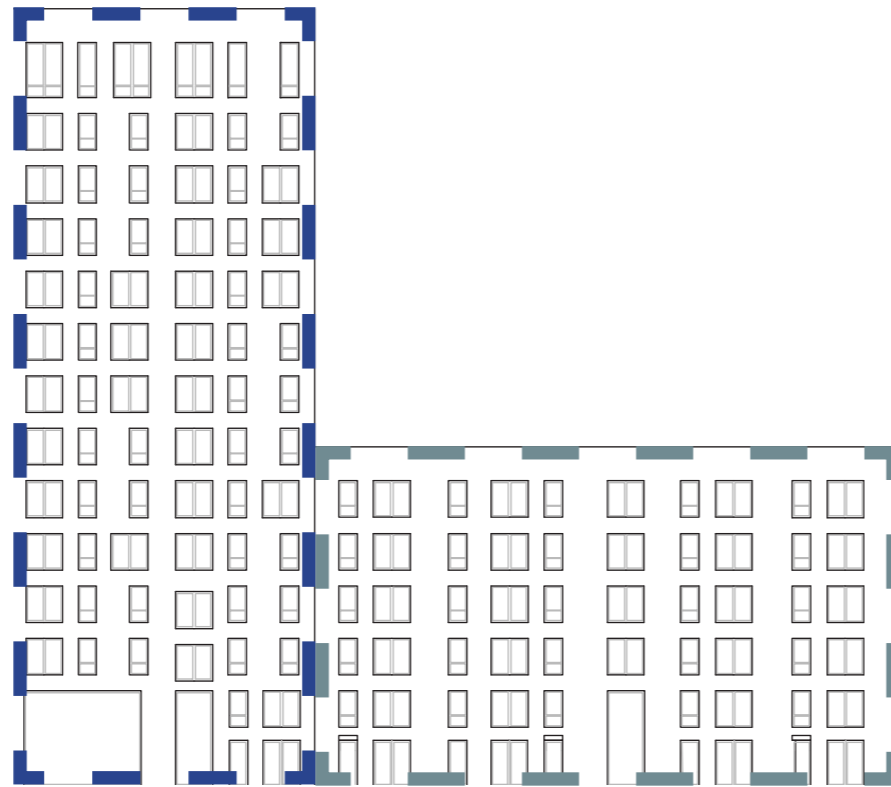
Block E (horizontal massing)

Block B Tower (vertical massing)

View from Western Road looking East at the shared zone between Block E and D (corner of Block E)



Block F



Block B (Tower)

Block B



Block E

Block D1

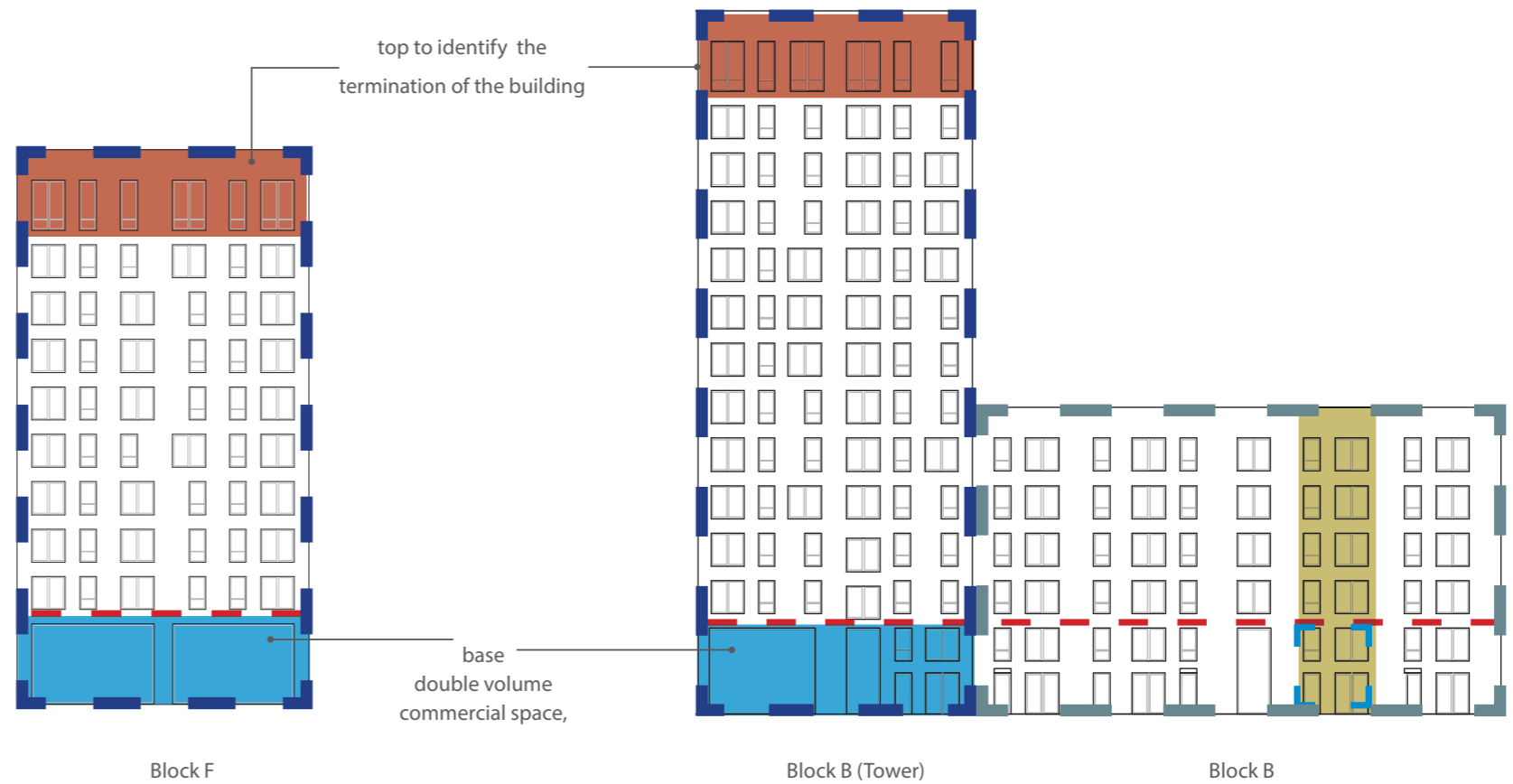
11.5 ELEVATION STUDIES - PROPORTIONS

The following design approach has been proposed for the following massing types:

- horizontal massing
- vertical massing

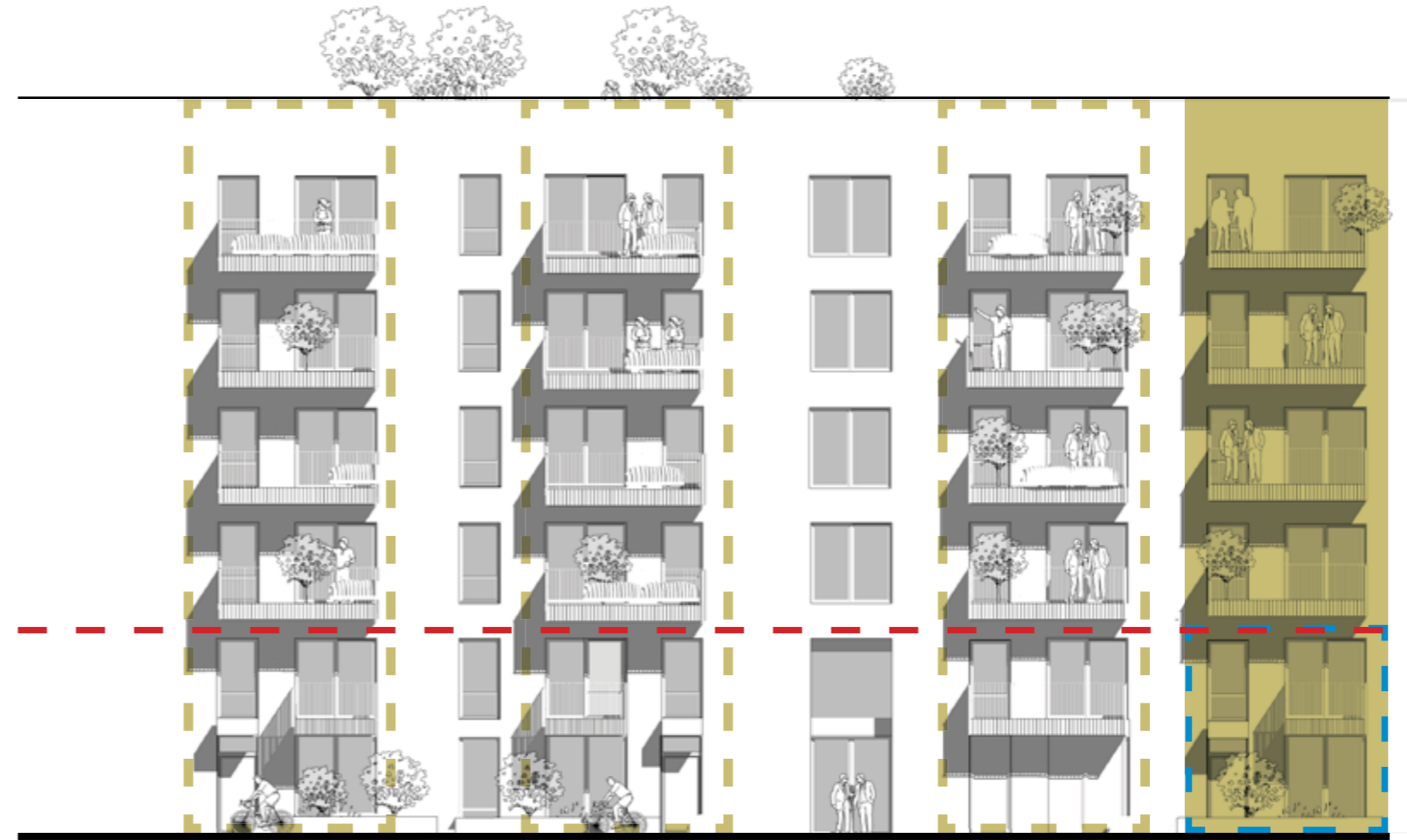
VERTICAL MASSING - BLOCK F AND BLOCK B TOWER

Articulation of the fenestration interrupted by playful movement. Top of the building identified by larger windows, treatment of double height commercial units on the ground floor enforce the base of the massing



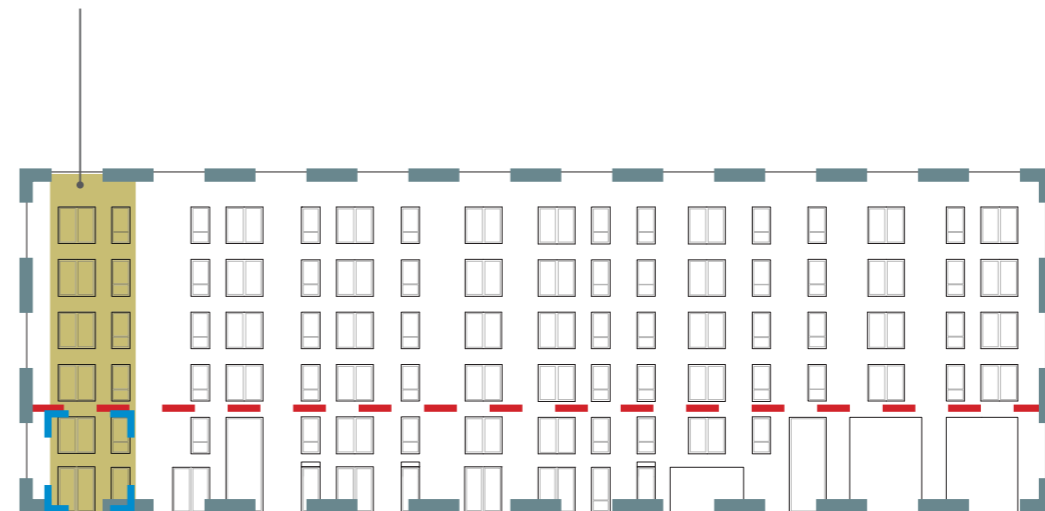
HORIZONTAL MASSING - BLOCK B AND BLOCK D1

Horizontal massing - clear articulation of the fenestration. Base of the massing identified by introducing a fine variation to fenestration and balcony treatment



Block B Elevation extract

Vertical rhythm



Block E

Block D1

11.6 ELEVATIONS - MATERIAL

BRICK

Brick is proposed as a facing material. This is to reflect the new London vernacular and provide a strong contextual relationship with the good quality historic brick buildings in the adjacent conservation area and throughout Wood Green. Brick as a robust material also relates to industrial heritage of the site. The texture and the colour of the brick is to be carefully selected to give a fresh and contemporary appearance to the design as well as to complement 1900's white brick building of The Chocolate Factory.

The 3no of different colours of facing brick have been explored for the development:

Option 1- grey brick

Option 2 - orange brick

Option 3 - mushroom brick

The two range of components finishes have been then applied for each of the brick options:

Range 1: anthracite window frame, anthracite cladding for balcony slab, anthracite canopy for private entrances

Range 2: light grey window frame, light grey aluminium cladding window reveals, decorative concrete balcony finish, light grey aluminium canopy for private entrances



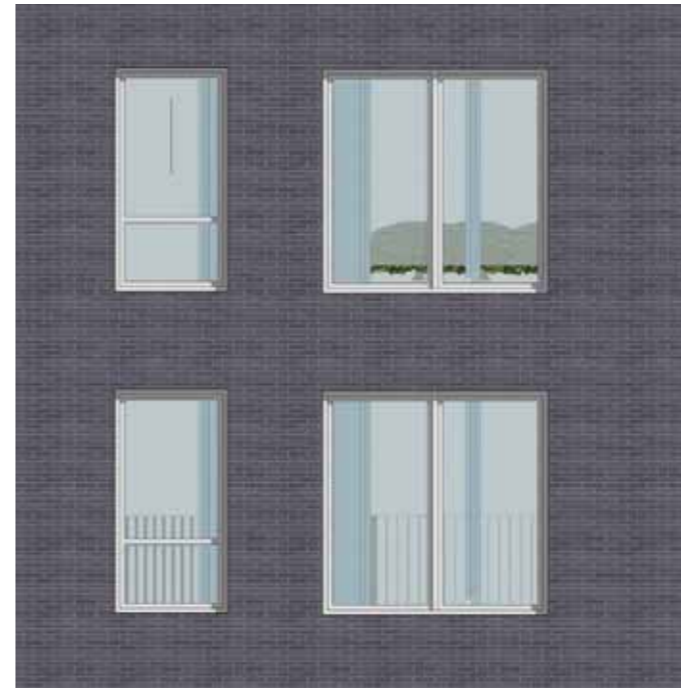
11.7 ELEVATION: MATERIAL COLOUR

OPTION 1: GREY BRICK

Colour chosen to complement The Chocolate Factory through a strong contrast

Grey brick in two tones: lighter and darker. Darker brick has been applied on tall blocks (Block B tower and Block F acting as a gateway) while lighter tone of the brick has been proposed for lower blocks.

The different brick tones has been matched with the two different colour range of components: anthracite and light grey



Variation 1: dark grey brick with contrasting light grey aluminium window frame and window reveal



Variation 2: lighter tone grey brick with contrasting anthracite grey aluminium window frame



View along Western Road looking North, West elevation of Block D1 and Block E

Metal railing balcony in two colour variant



Variation 1: contrasting light grey balcony, concrete slab



Variation 2: contrasting anthracite balcony



View into the proposal from Western Road, Block F, Block E and the corner of The Chocolate Factory

OPTION 2: ORANGE BRICK



Variation 1: contrasting light grey aluminium window frame and window reveal



Variation 2: contrasting anthracite aluminium window frame



View along Western Road looking North, West elevation of Block D1 and Block E

Metal railing balcony in two colour variant



Deep light grey window reveal with light grey window frame (aluminium cladding)

Matching light grey balcony

Variation 1: contrasting light grey balcony, concrete slab

Variation 2: contrasting anthracite balcony



View into the proposal from Western Road, Block F, Block E and the corner of The Chocolate Factory

OPTION 3: MUSHROOM BRICK - PREFERRED



Variation 1: contrasting light grey aluminium window frame and window reveal



Variation 2: contrasting anthracite aluminium window frame



View along Western Road looking north, West elevation of Block D1 and Block E

Metal railing balcony in two colour variant



Deep light grey window reveal with light grey window frame (aluminium cladding)

Matching light grey balcony

Variation 1: contrasting light grey balcony, concrete slab

Variation 2: contrasting anthracite balcony



View into the proposal from Western Road, Block F, Block E and the corner of The Chocolate Factory



view A

Block F

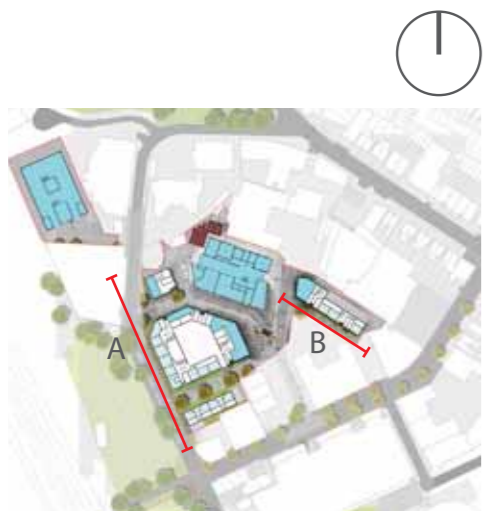
Block E

Block D1



view B

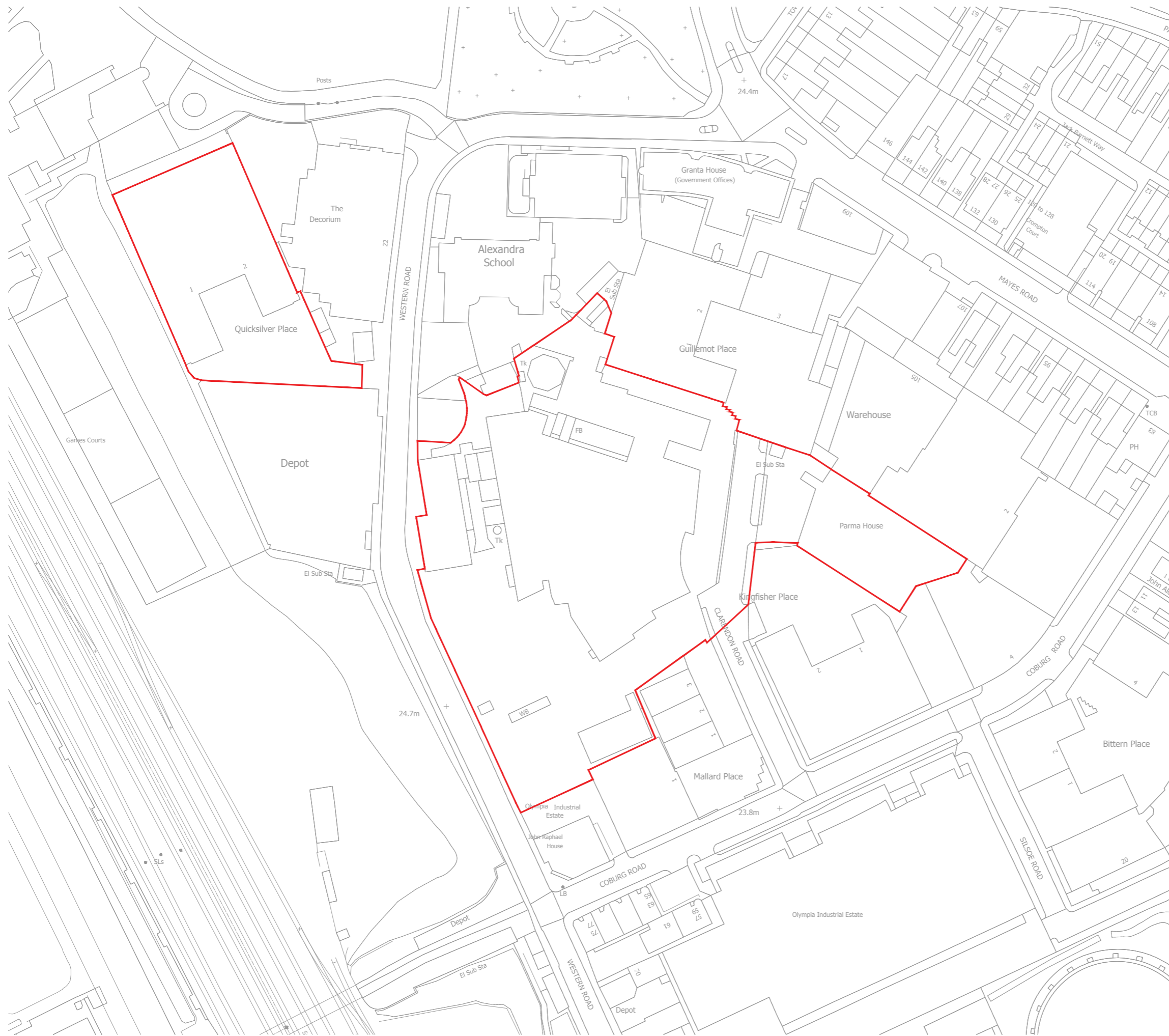
Block B







12 DRAWING SET FOR DETAILED PLANNING APPLICATION



 Detailed Application Boundary Line

Project
**The Chocolate Factory
 Haringey**

Drawing Title
**Red Line Drawing
 Detailed Application**

Date	Scale	Drawn by	Check by
09.11.15	1:1250@A3	AB	PJ
Project No	Drawing No	Revision	
25021	PL-001	-	



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LEGEND



PRELIMINARY ISSUE ONLY,
SUBJECT TO DESIGN DEVELOPMENT.

Project
The Chocolate Factory Wood Green
 Drawing Title
Site Plan, Illustrative Masterplan Ground Floor
 Date
 23.09.2015
 Scale
 1:500@A1
 Project No
 25021
 Drawn by
 RA
 Drawing No
 SK011
 Check by
 MK
 Revision
 A



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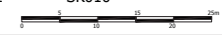
LEGEND



PRELIMINARY ISSUE ONLY,
 SUBJECT TO DESIGN DEVELOPMENT.

Project
**The Chocolate Factory
 Wood Green**
 Drawing Title
**Site Plan, Detailed Application
 Ground Floor**

Date 23.09.2015	Scale 1:500@A1	Drawn by RA	Check by MK
Project No 25021	Drawing No SK010	Revision A	

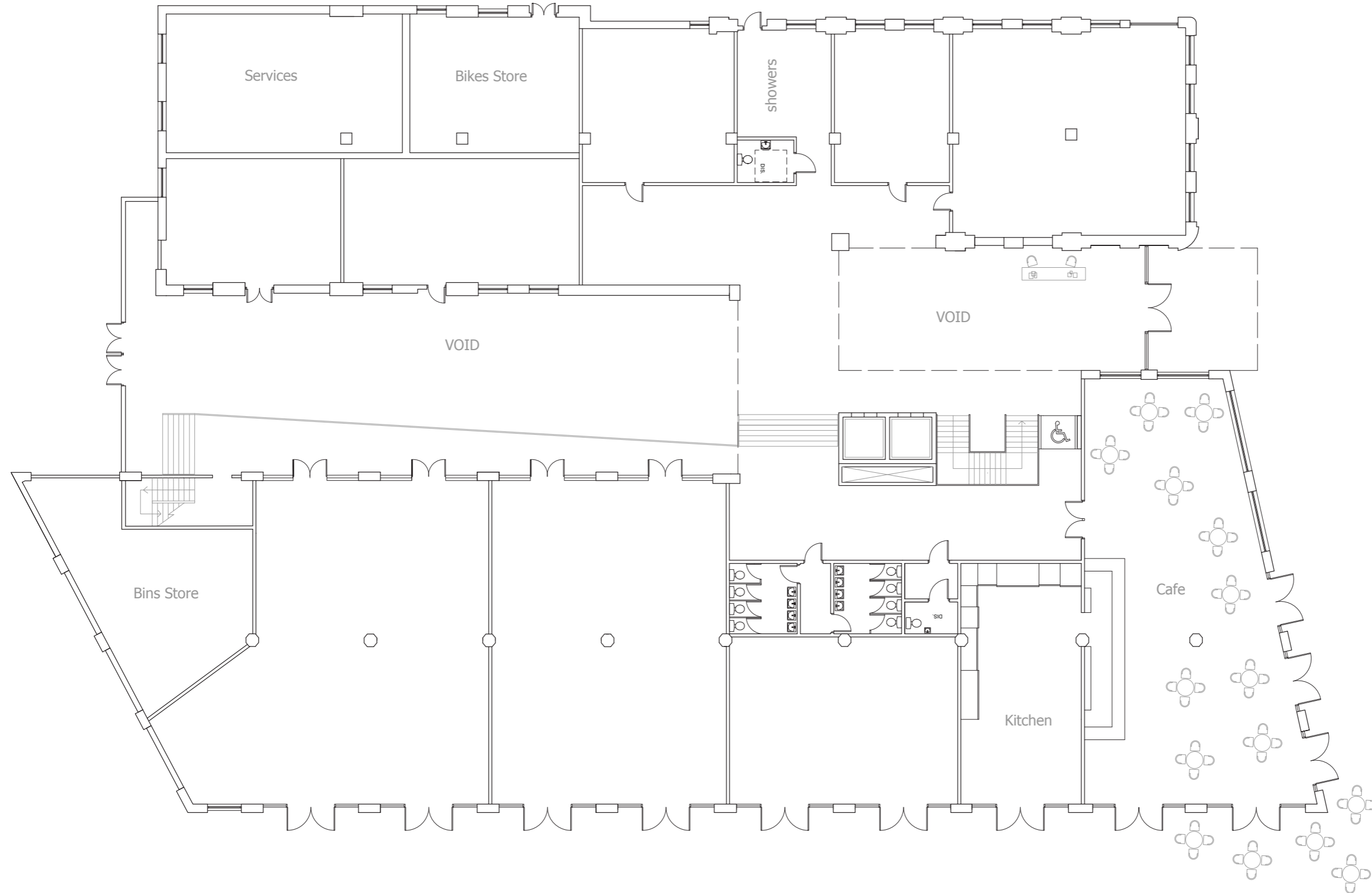
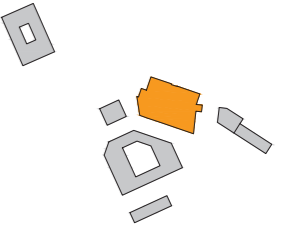


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PRELIMINARY ISSUE ONLY,
SUBJECT TO DESIGN DEVELOPMENT
AND BUILDING SURVEY. LEVELS SHOWN
INDICATIVELY ONLY AND TO BE REVIEWED
WHEN SURVEY INFORMATION IS AVAILABLE.



Project
**Wood Green
The Chocolate Factory**
Drawing Title
**Block A Plans
Ground Floor**

Date	Scale	Drawn by	Check by
15.12.15	1:100@A1	RA	MK
Project No	Drawing No	Revision	
25021	SK BA 100		



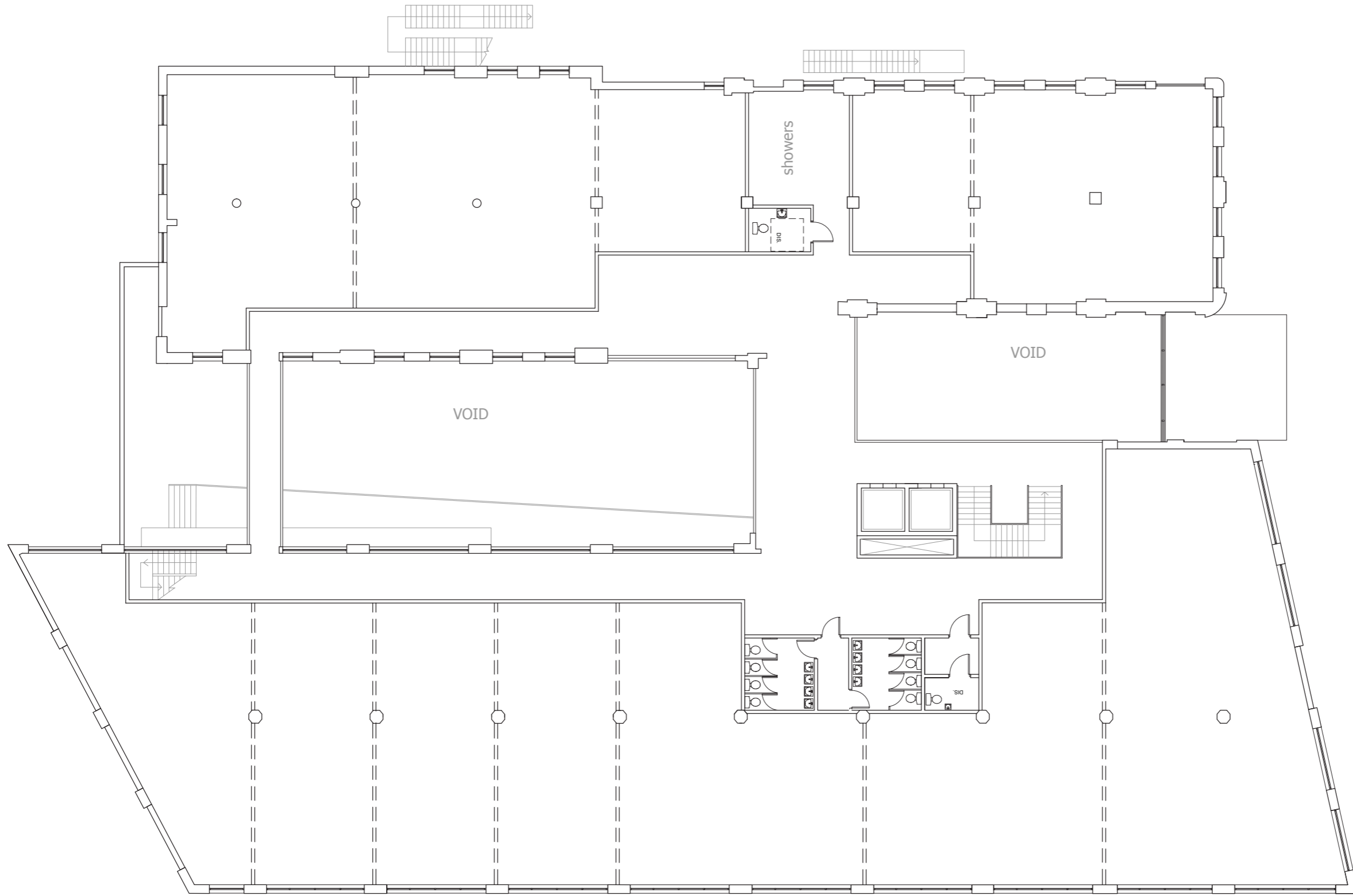
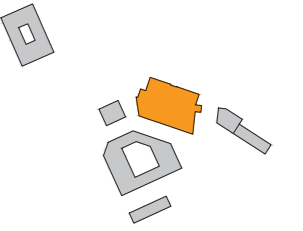
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AND BUILDING SURVEY. LEVELS SHOWN
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Project
**Wood Green
The Chocolate Factory**

Drawing Title
**Block A Plans
First Floor Plan**

Date	Scale	Drawn by	Check by
15.12.15	1:100@A1	RA	MK
Project No	Drawing No	Revision	
25021	SK BA 101		



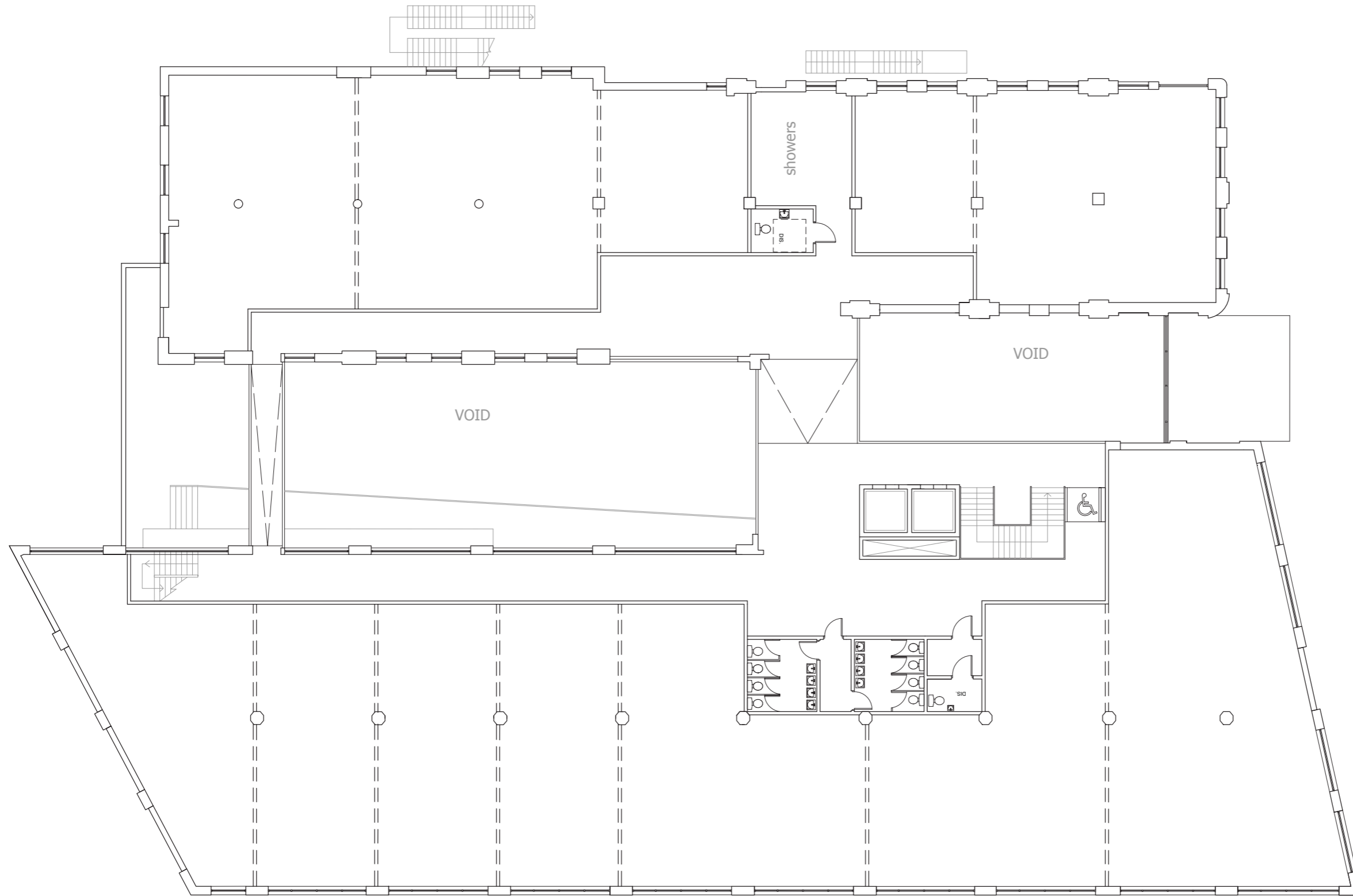
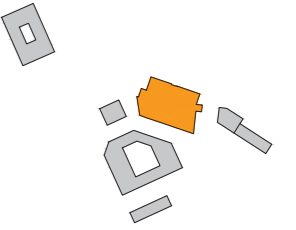
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AND BUILDING SURVEY. LEVELS SHOWN
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Project
**Wood Green
The Chocolate Factory**
Drawing Title
**Block A Plans
Second Floor Plan**

Date	Scale	Drawn by	Check by
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Project No	Drawing No	Revision	
25021	SK BA 102		



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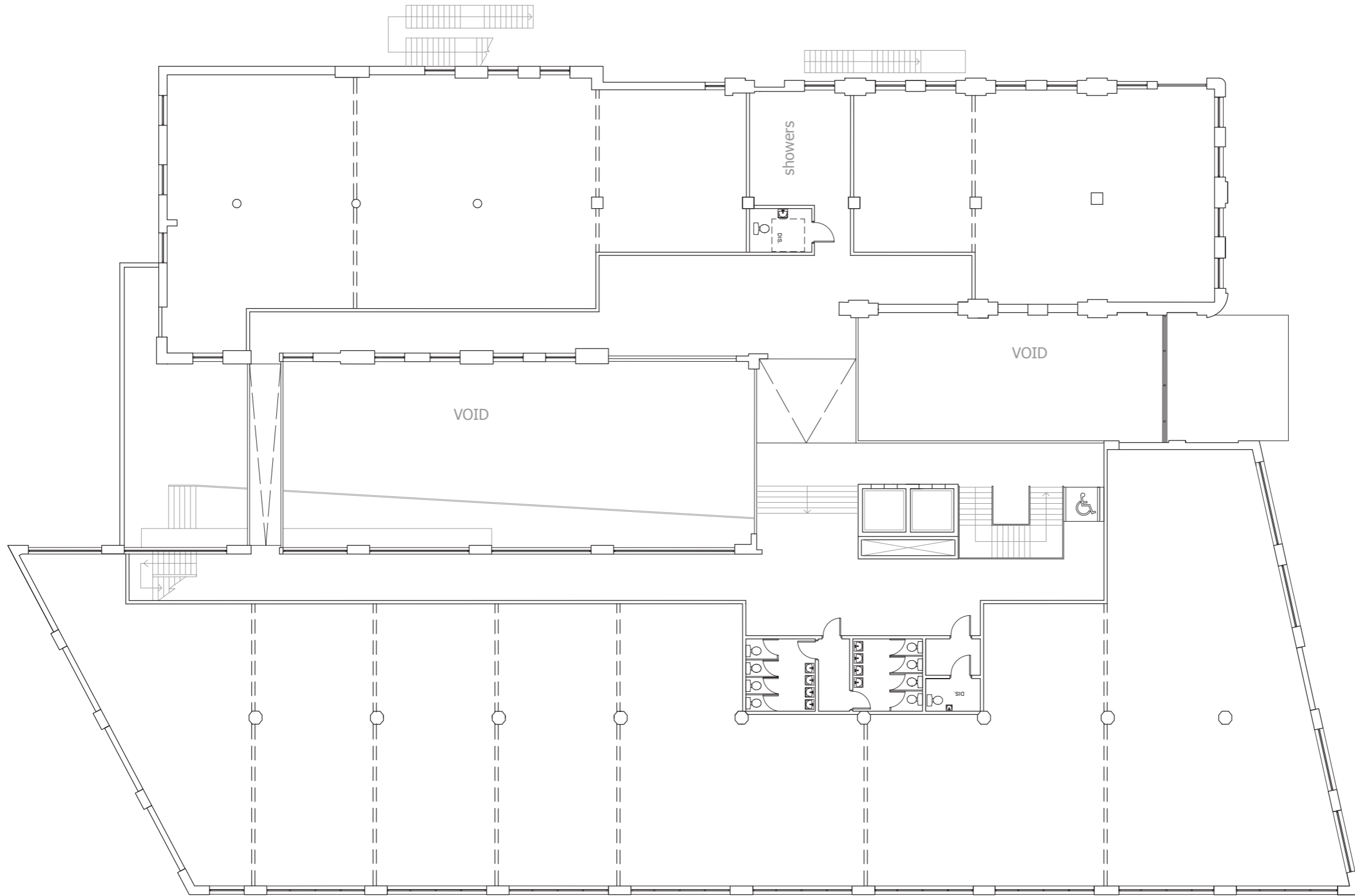
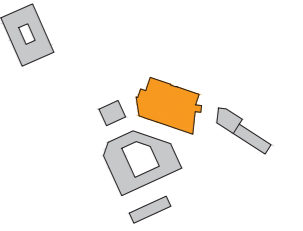
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LEGEND



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Project
**Wood Green
The Chocolate Factory**

Drawing Title
**Block A Plans
Third Floor Plan**

Date 15.12.15	Scale 1:100@A1	Drawn by RA	Check by MK
Project No 25021	Drawing No SK BA 103	Revision	



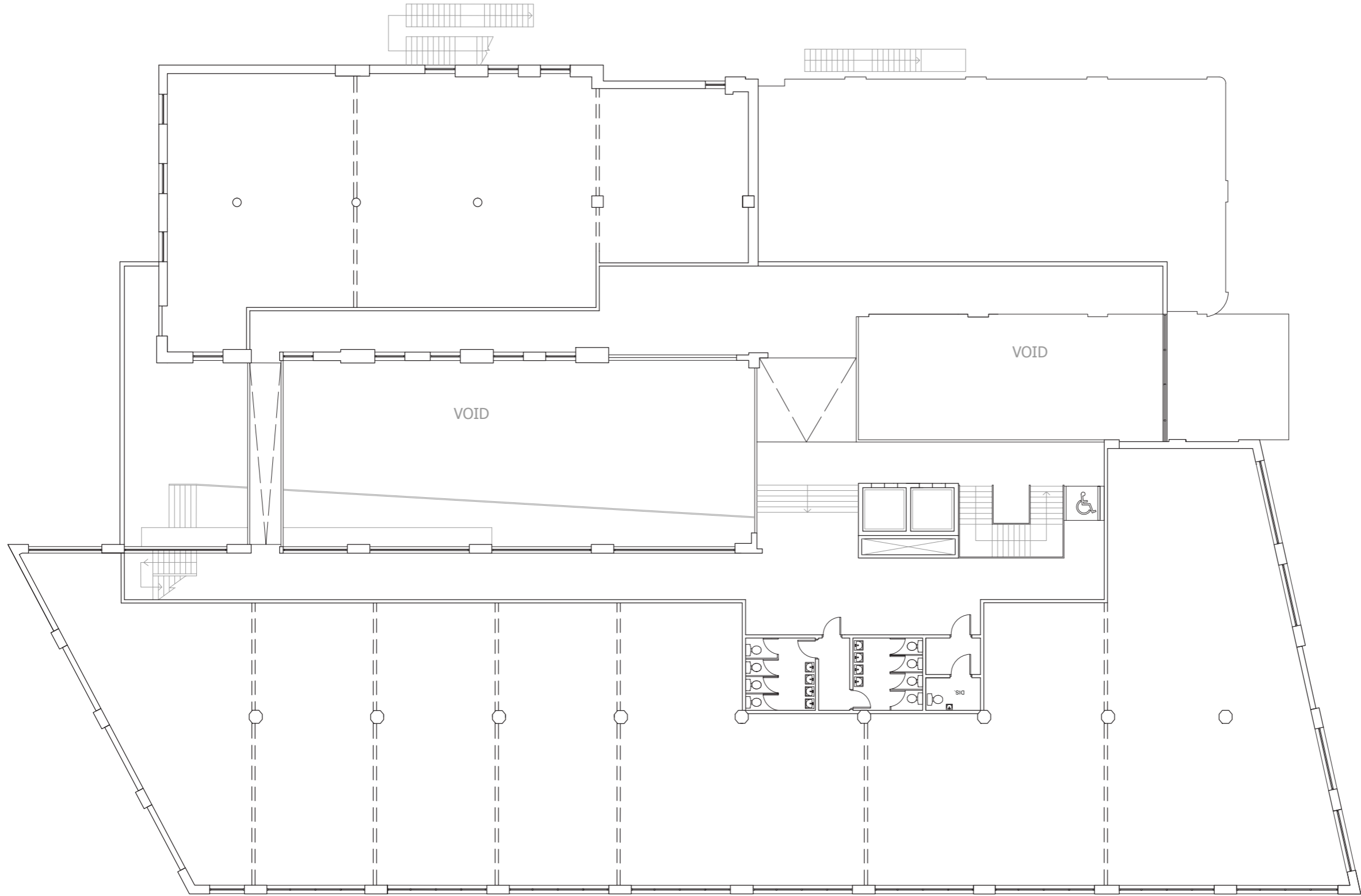
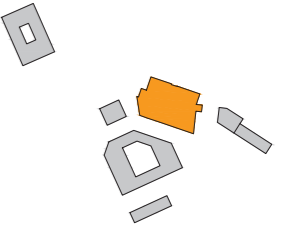
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Project
**Wood Green
The Chocolate Factory**
Drawing Title
**Block A Plans
Fourth Floor Plan**

Date	Scale	Drawn by	Check by
15.12.15	1:100@A1	RA	MK
Project No	Drawing No	Revision	
25021	SK BA 104		



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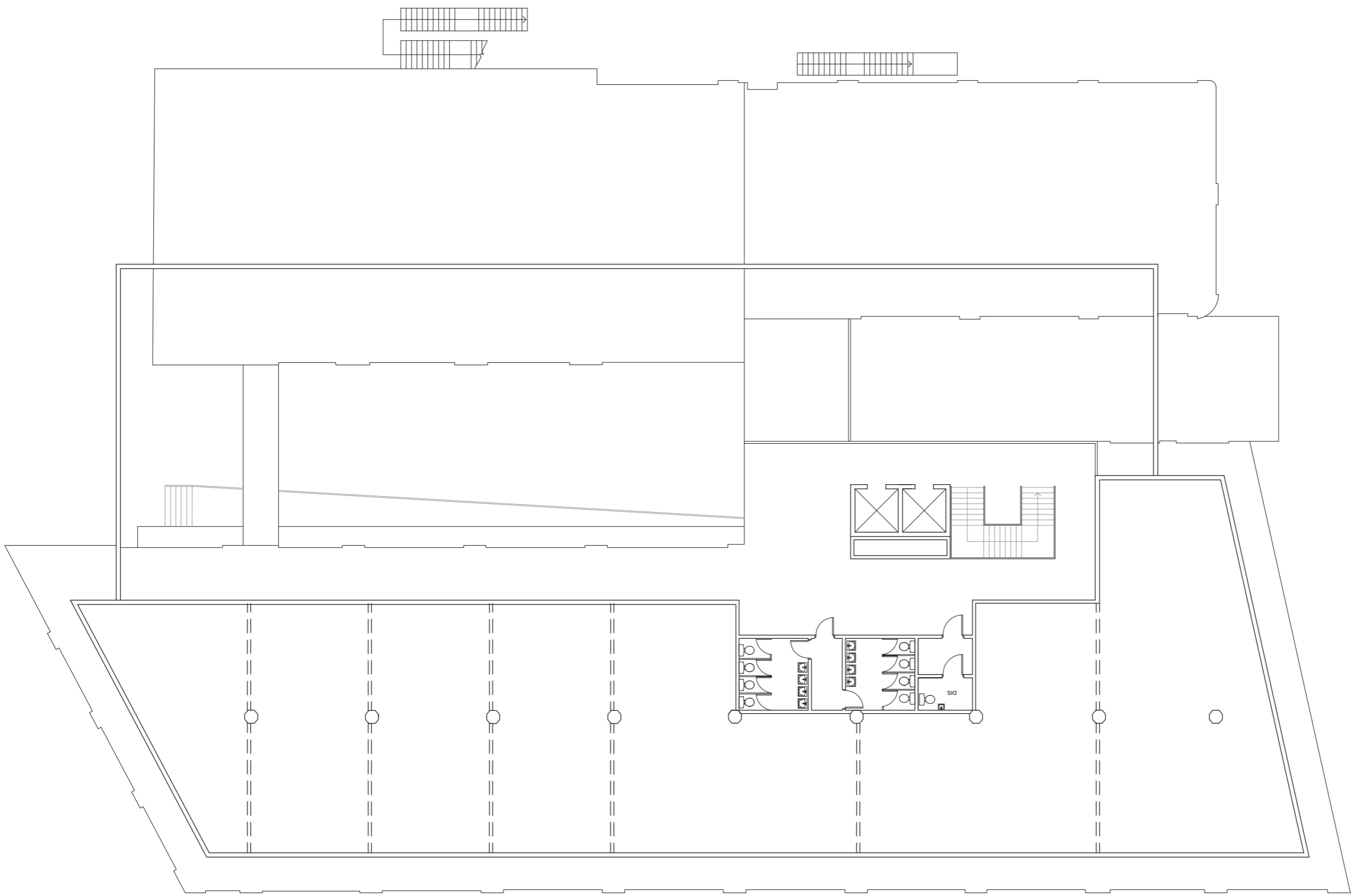
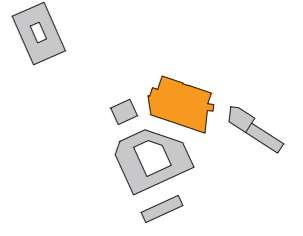
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Project
Wood Green
The Chocolate Factory

Drawing Title
Block A Plans
Fifth Floor Plan

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Project No	Drawing No	Revision	
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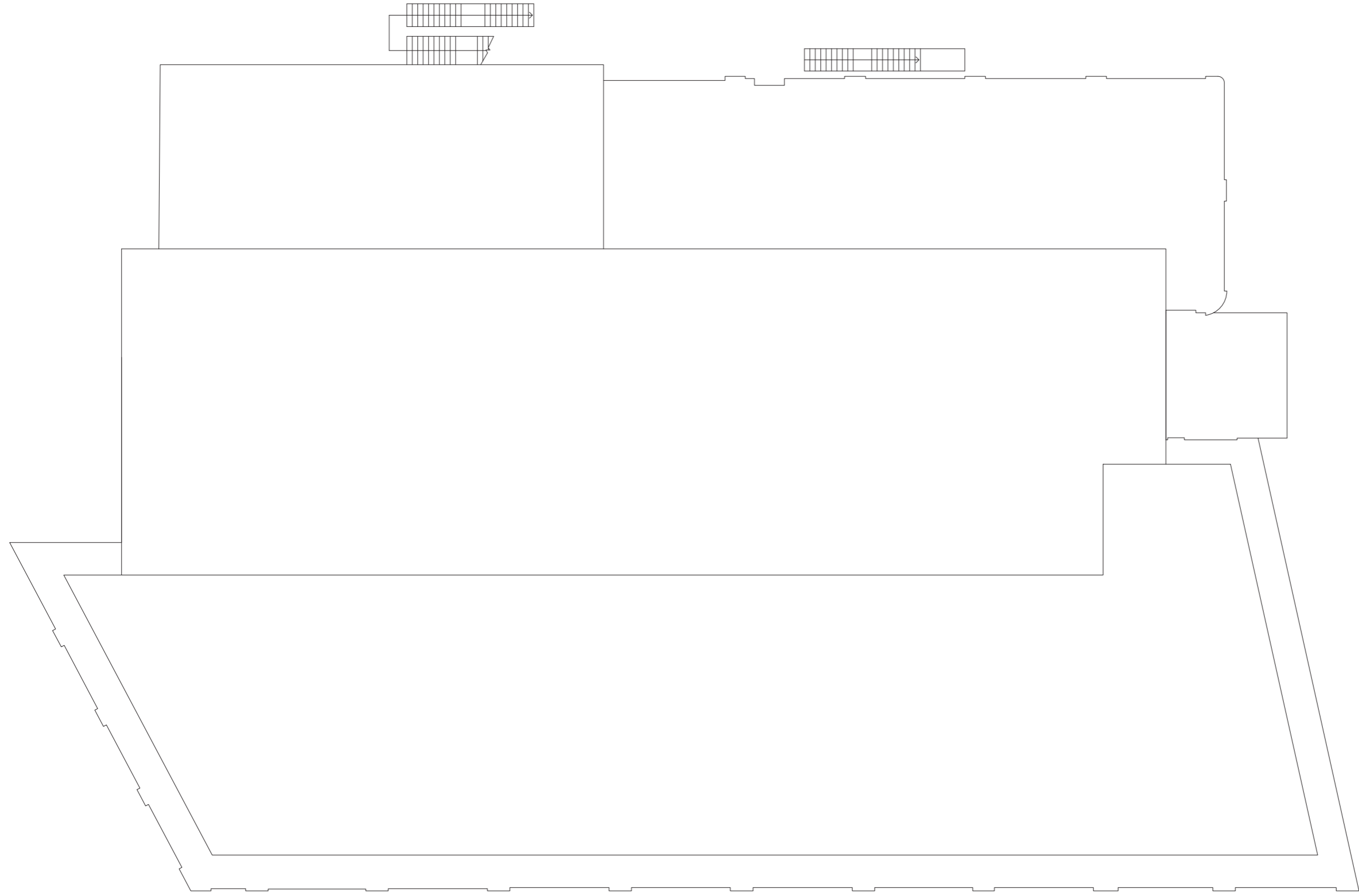
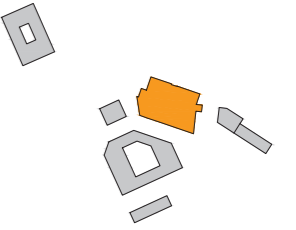


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Project
Wood Green
The Chocolate Factory
Drawing Title
Block A Plans
Roof Plan

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Project No	Drawing No	Revision	
25021	SK BA 106		

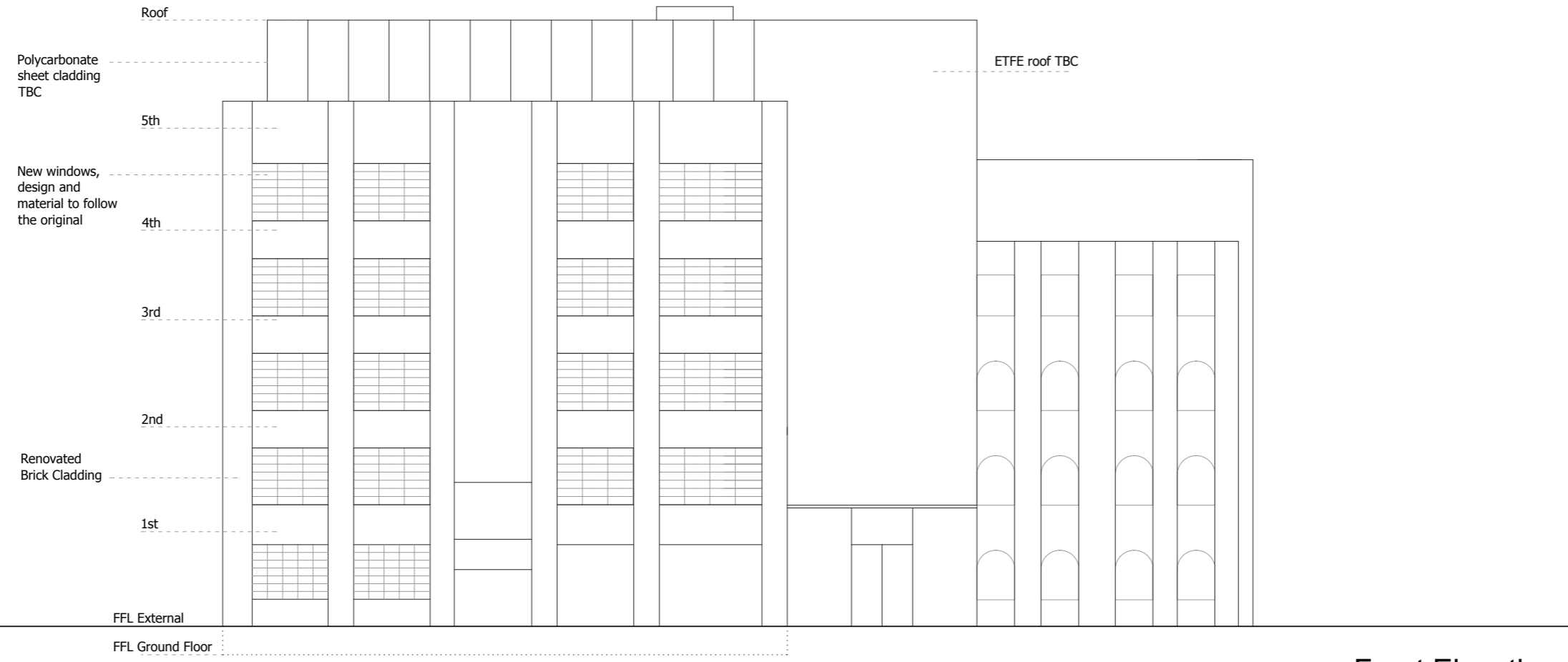
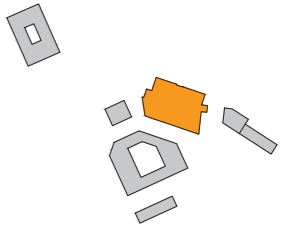


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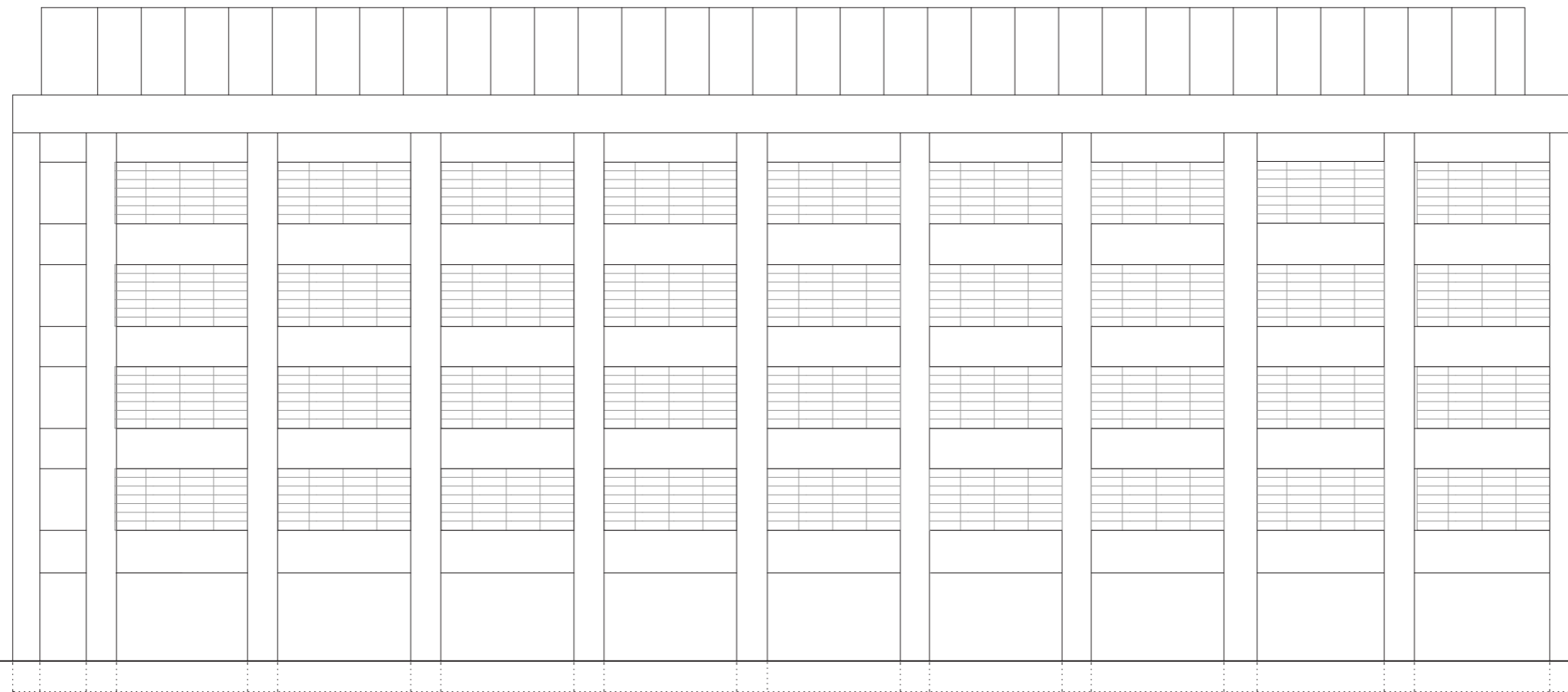
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Front Elevation



South Elevation

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Project
**Wood Green
 The Chocolate Factory**

Drawing Title
**Block A Elevations
 Elevations**

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 Project No 25021 Drawing No SK BA 200 Revision

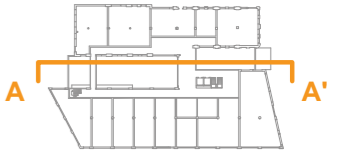
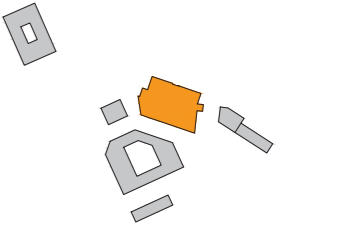


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Section A-A'

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Project
**Wood Green
The Chocolate Factory**
Drawing Title
**Block A Sections
Section A - A'**

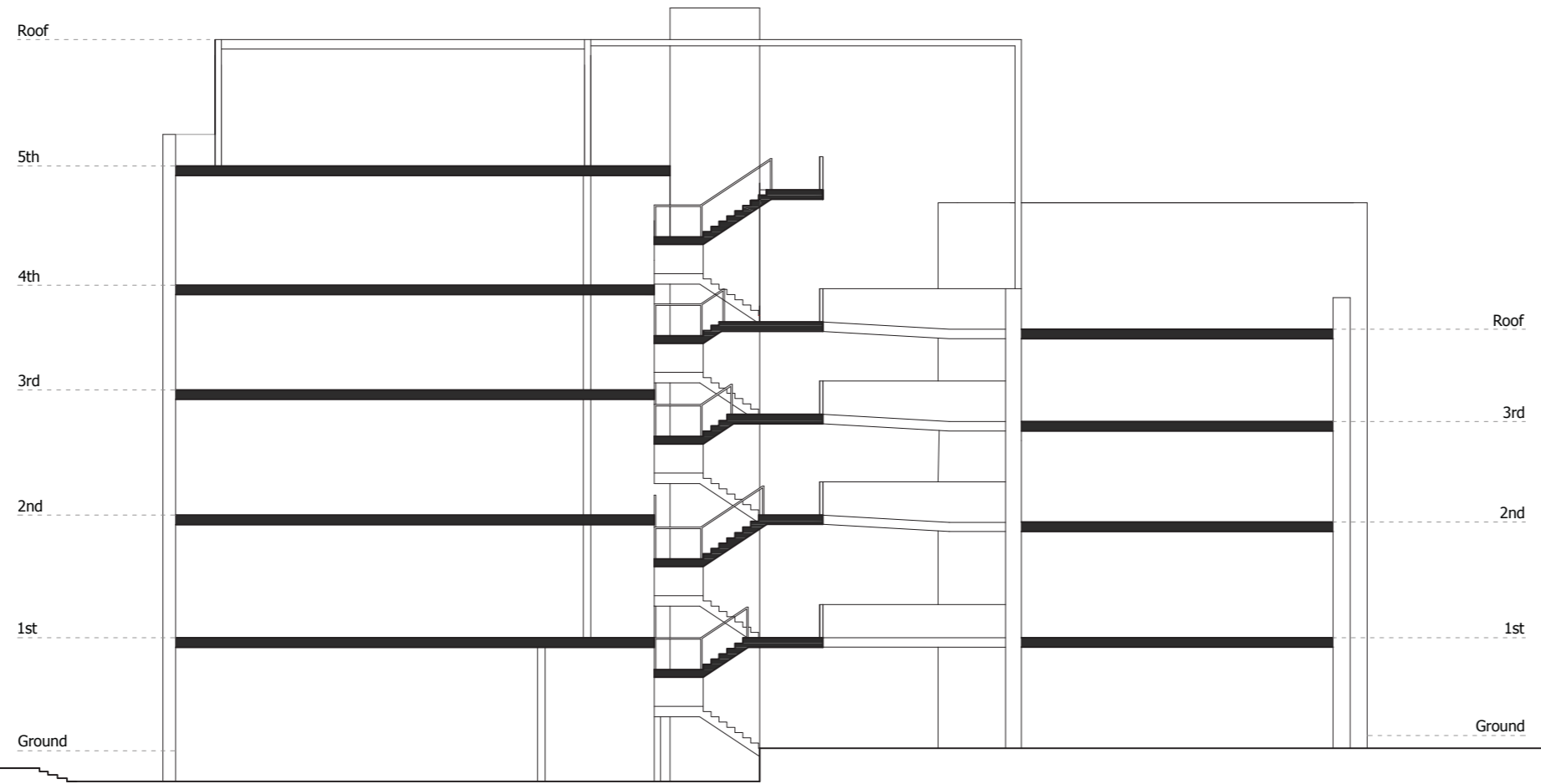
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Project No	Drawing No	Revision	
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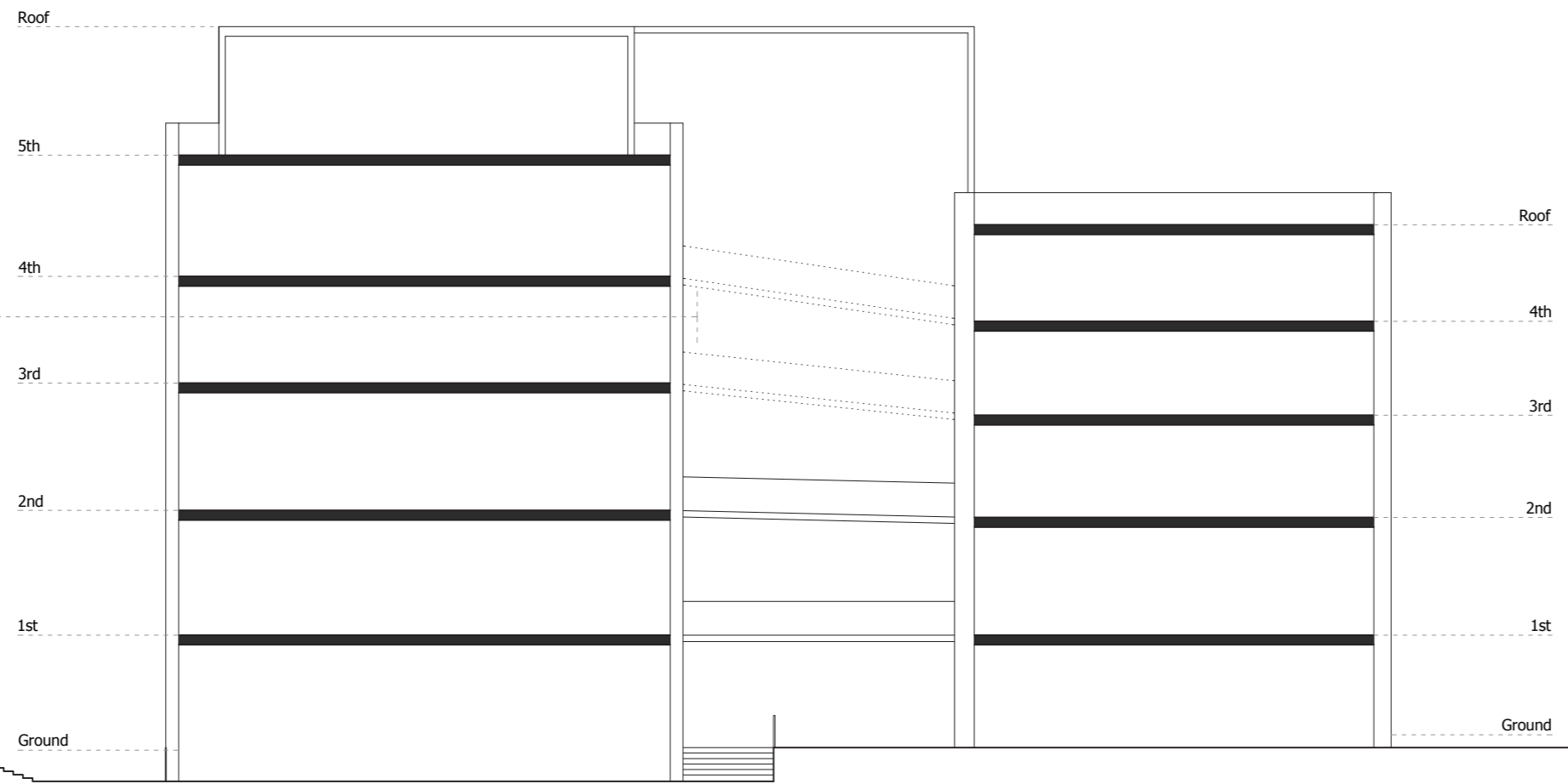
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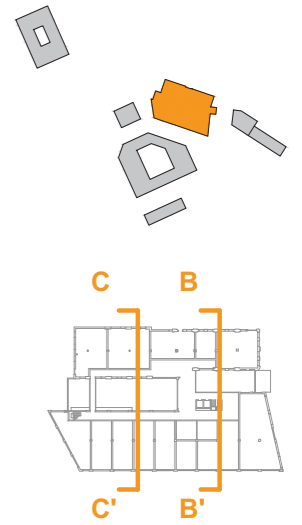


Section B-B'



Section C-C'

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Project
Wood Green
 The Chocolate Factory
 Drawing Title
Block A Sections
 Sections B-B' and C-C'

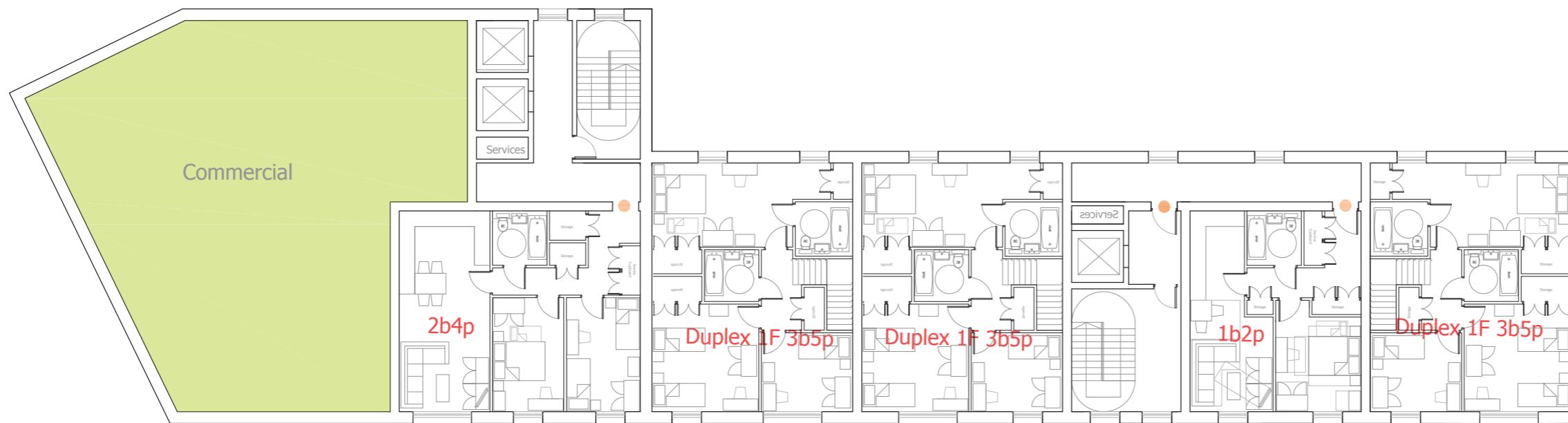
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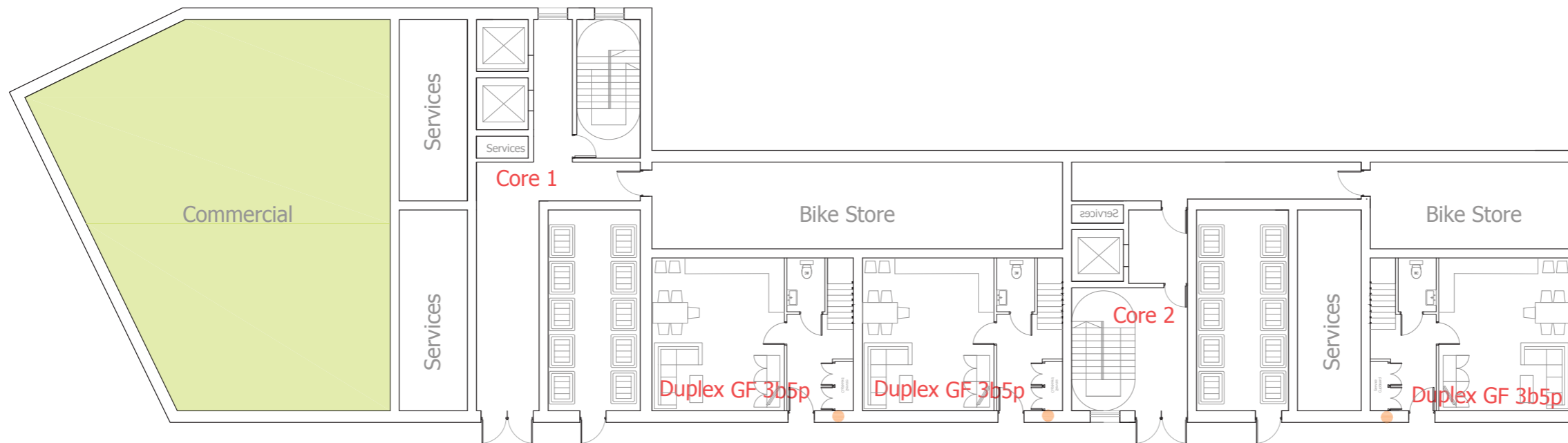
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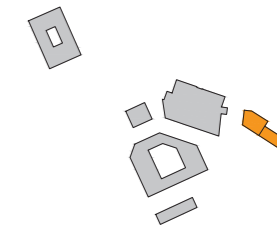


1st Floor Plan



Ground Floor Plan

LEGEND



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Project
The Chocolate Factory
 Wood Green

Drawing Title
Block B Plans
 Ground and First Floor Plan

Date	Scale	Drawn by	Check by
06.11.15	1:100@A1	RA	MK
Project No	Drawing No	Revision	
25021	SK BB 100	A	



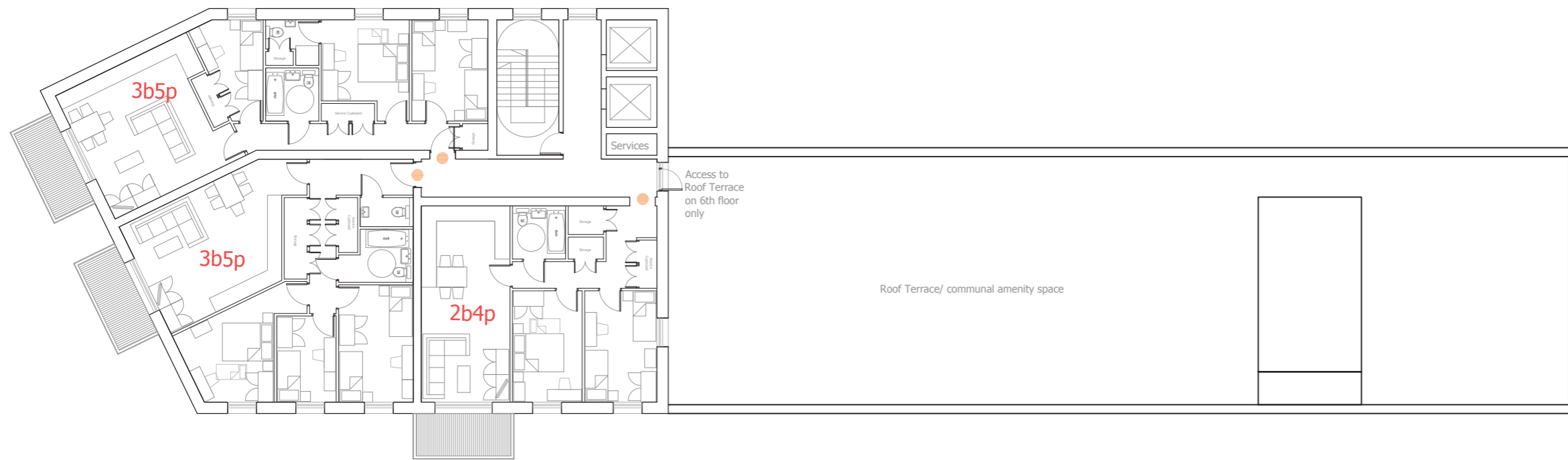
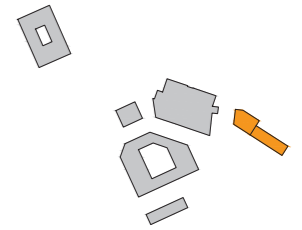
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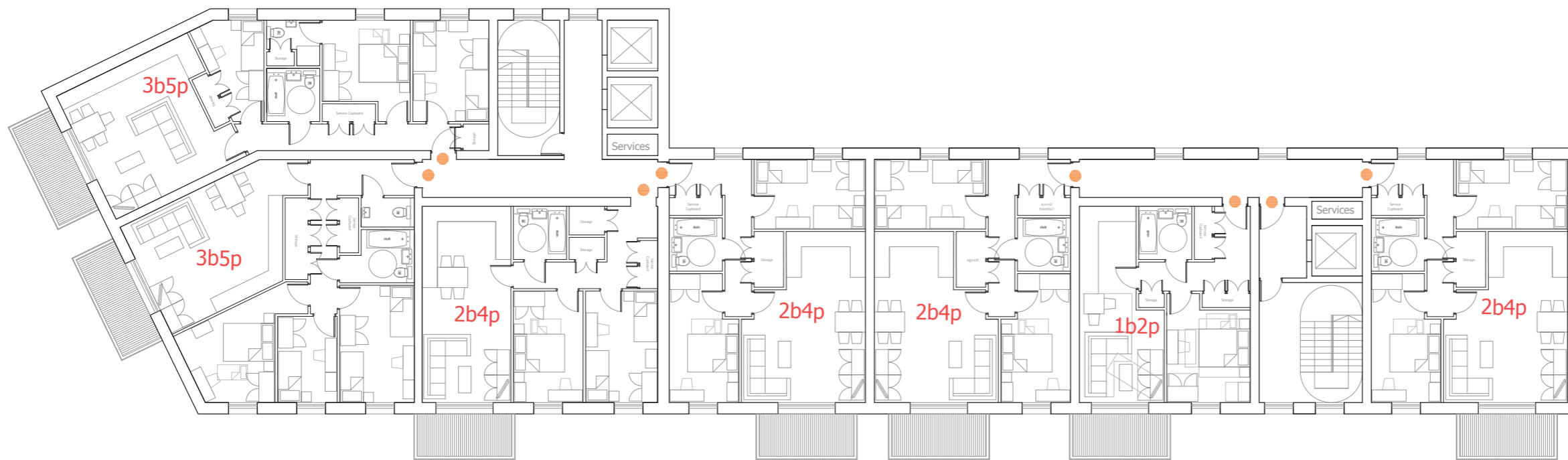
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6th- 13 Floor Plan



2nd - 5th Floor Plan

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Project
**The Chocolate Factory
 Wood Green**
 Drawing Title
**Block B Plans
 2nd - 13th Floor Plan**

Date	Scale	Drawn by	Check by
06.11.15	1:100@A1	RA	MK
Project No	Drawing No	Revision	
25021	SK BB 101	-	



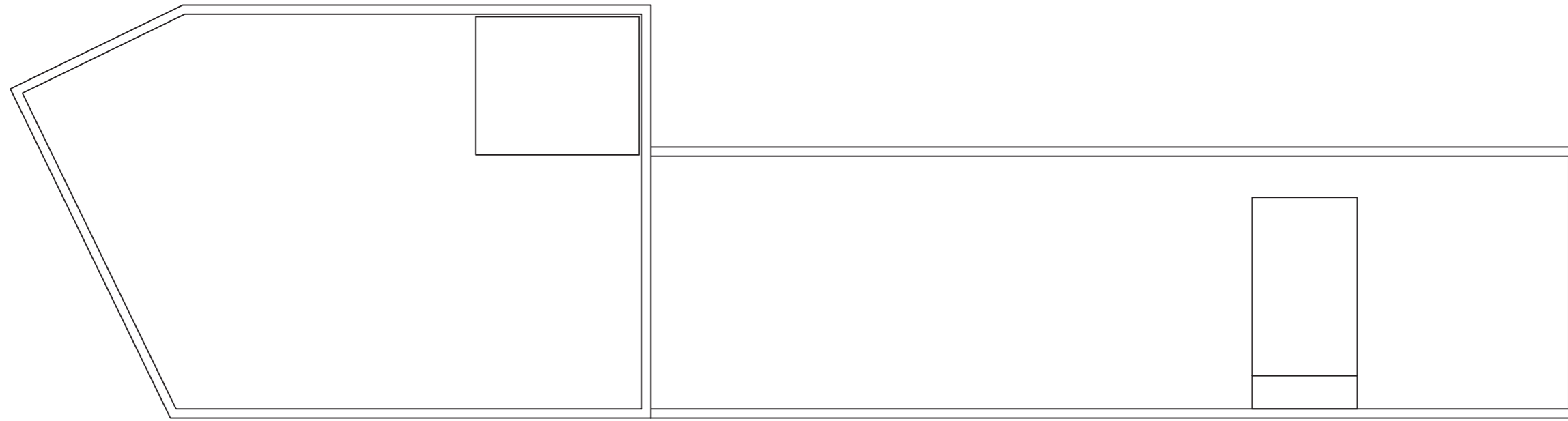
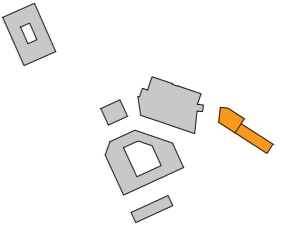
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Roof Plan

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Project
**The Chocolate Factory
Wood Green**
Drawing Title
**Block B Plans
Roof Plan**

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06.11.15	1:100@A1	RA	MA
Project No	Drawing No	Revision	
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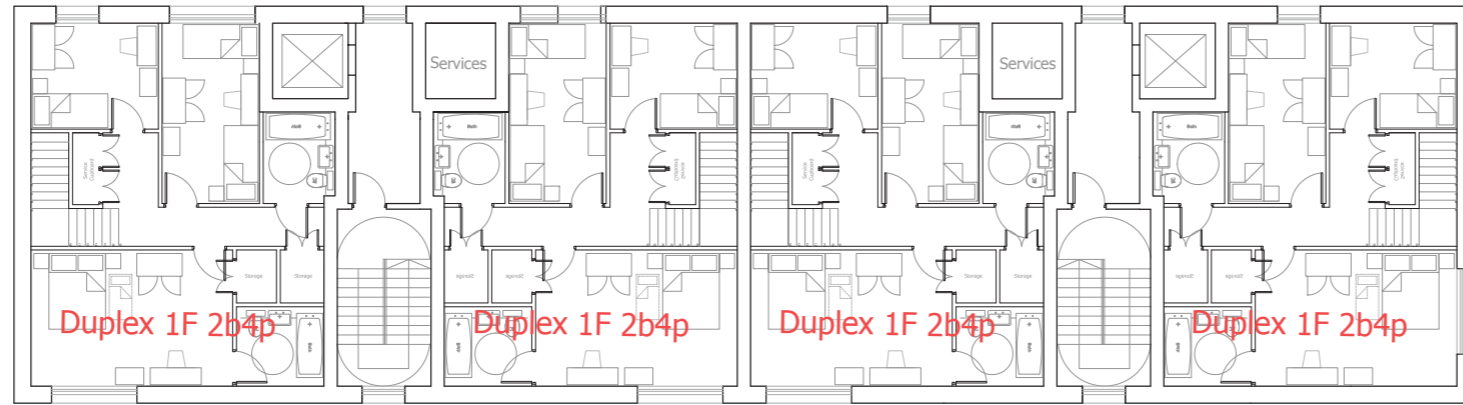
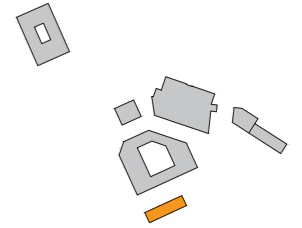
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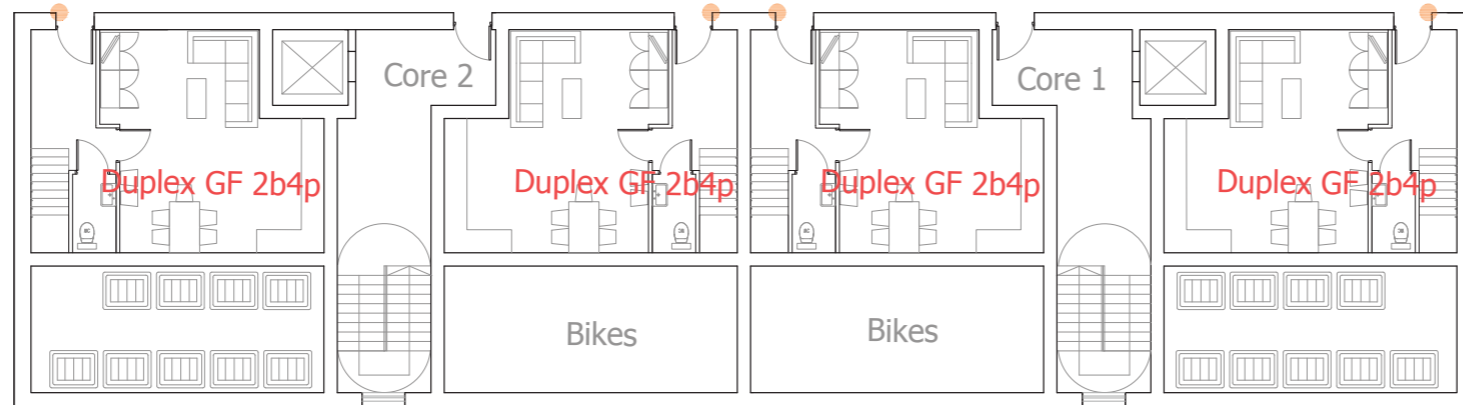
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LEGEND



First Floor



Ground Floor

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Project
**The Chocolate Factory
 Wood Green**
 Drawing Title
**Block D1 Plans
 Ground and 1st Floor**

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06/11/15	1:100@A1	RA	MK
Project No	Drawing No	Revision	
25021	SK BD1 100	A	

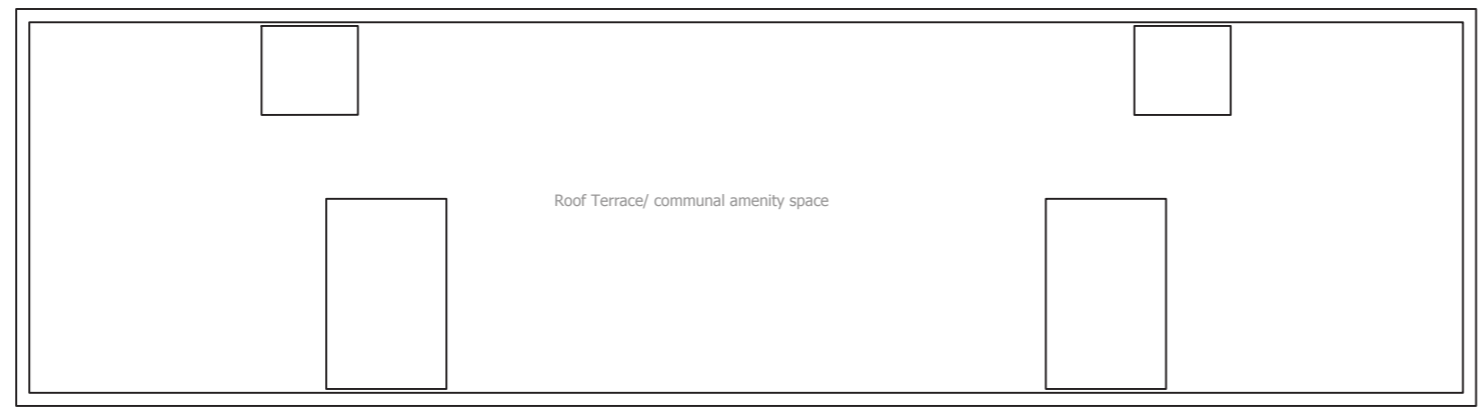
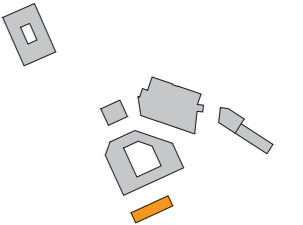


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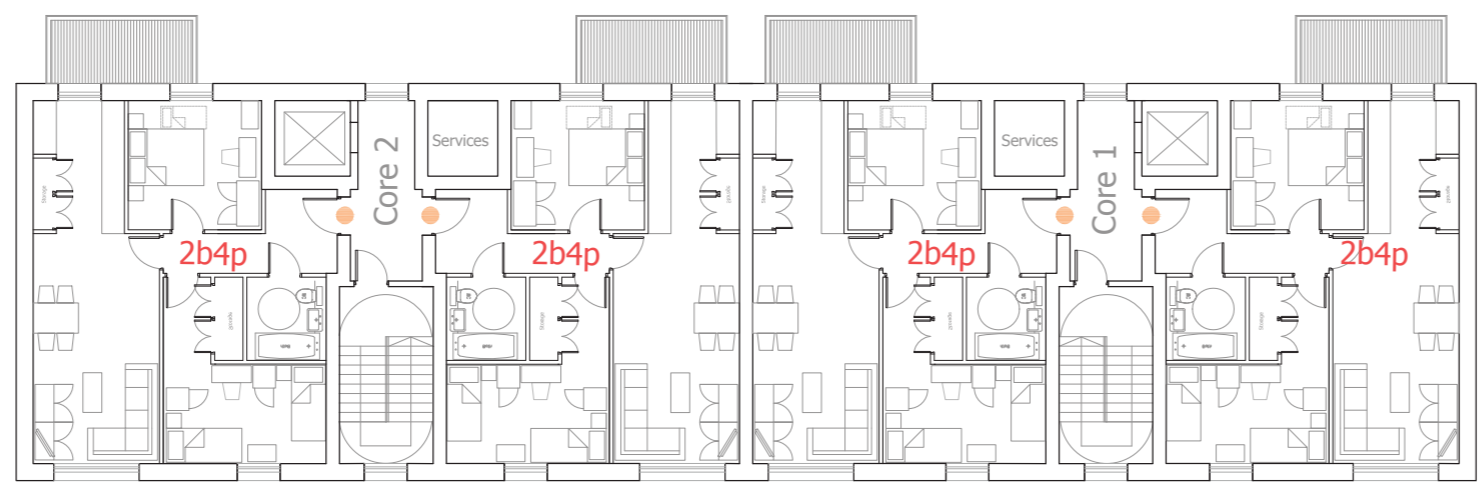
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Roof Plan



2nd-5th Floor

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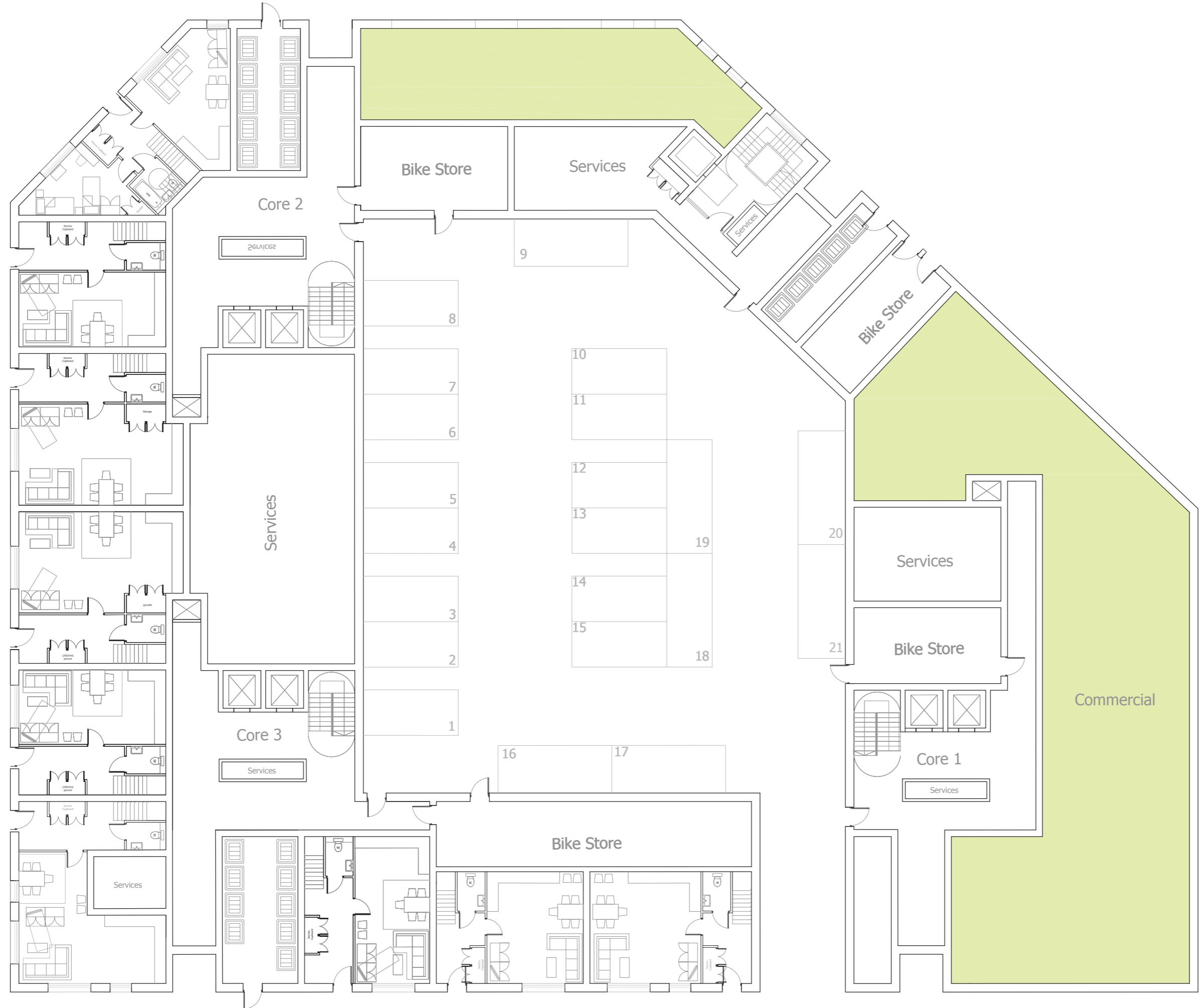
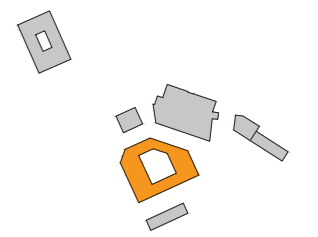
Project
**The Chocolate Factory
 Wood Green**
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**Block D1 Plans
 2th-5th Floor and Roof Plan**
 Date Scale Drawn by Check by
 06/11/15 1:100@A1 RA MK
 Project No Drawing No Revision
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Project
**The Chocolate Factory
 Wood Green**
 Drawing Title
**Block E Plans
 Ground Floor**

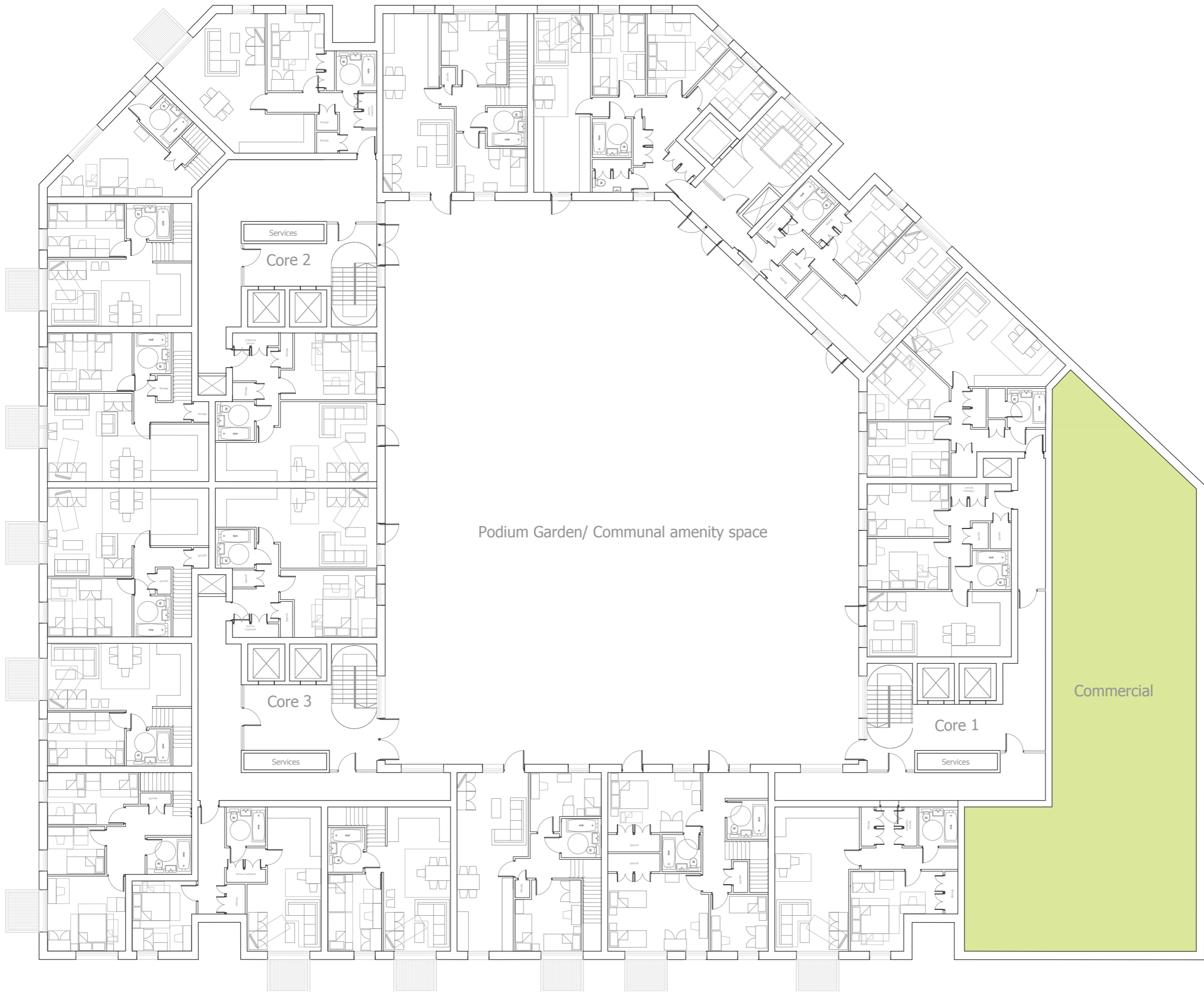
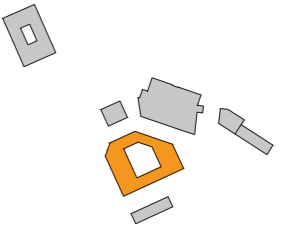
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Project
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 Drawing Title
**Block E Plans
 1st Floor Plan**

Date	Scale	Drawn by	Check by
09/11/15	1:100@A1	RA	MK
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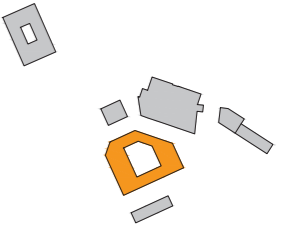
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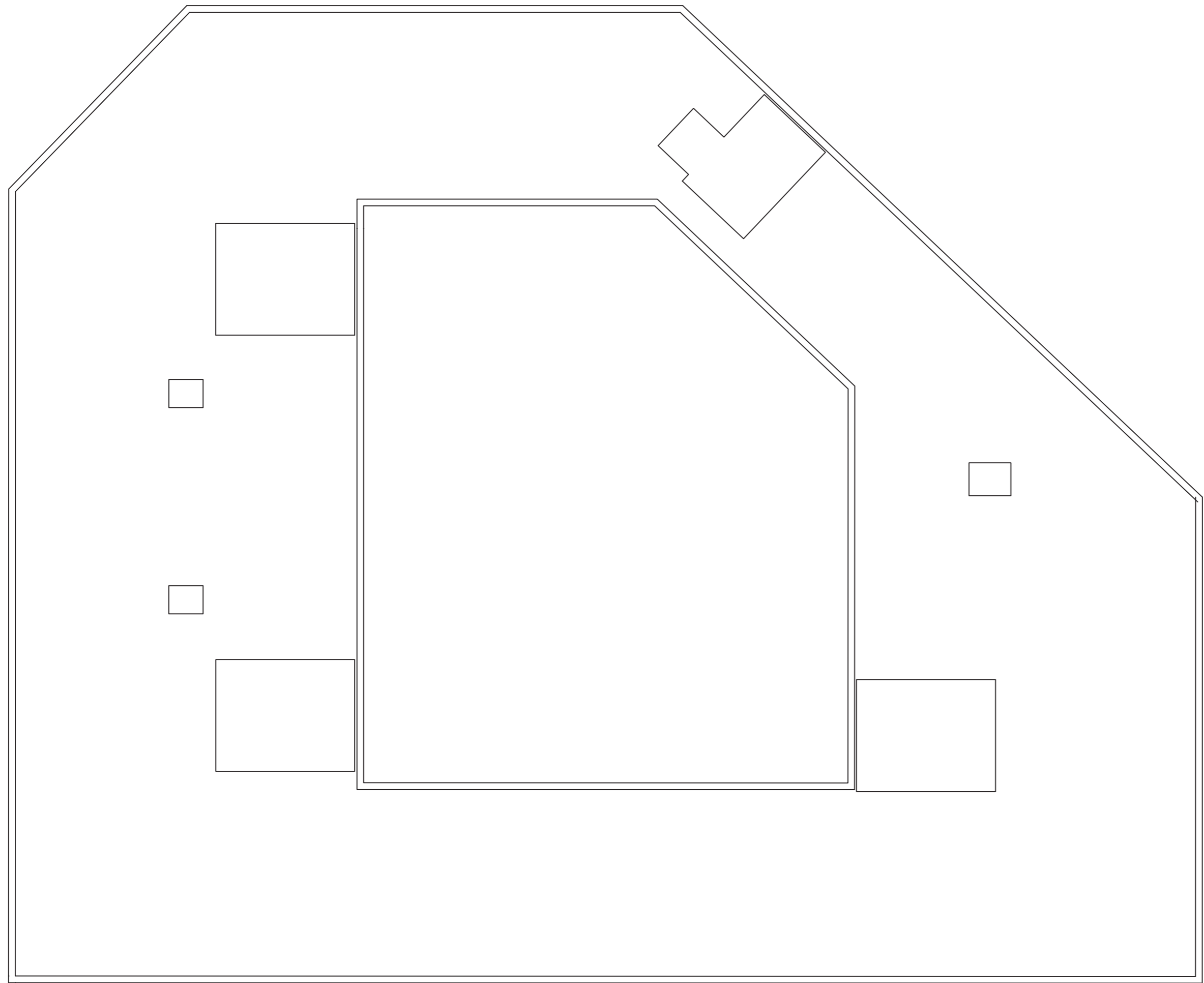
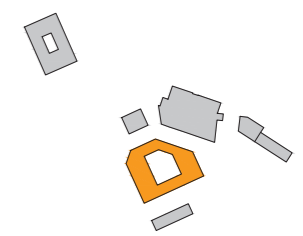


Project
**The Chocolate Factory
 Wood Green**
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 Date 09/11/15 Scale 1:100@A1 Drawn by RA Check by MK
 Project No 25021 Drawing No SK BE 102 Revision -

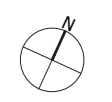


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Project
**The Chocolate Factory
Wood Green**

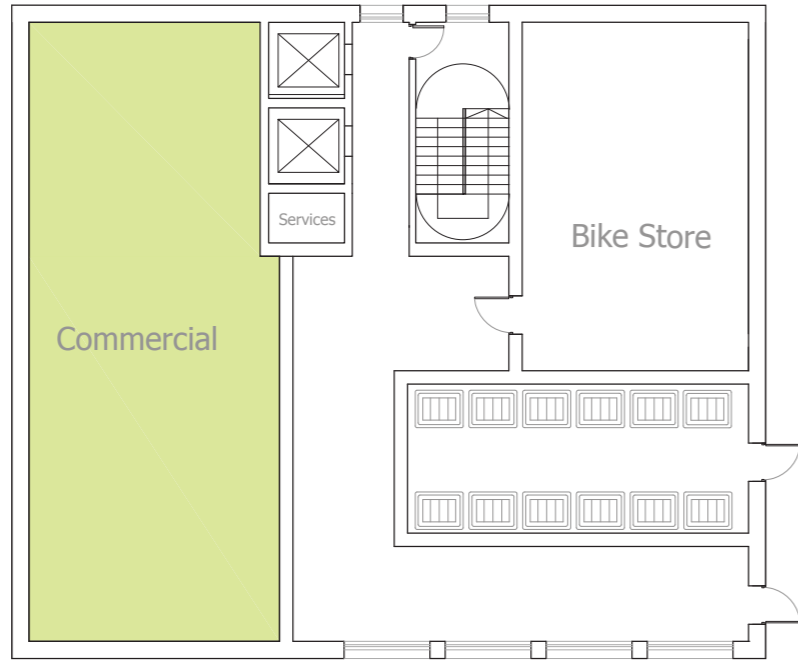
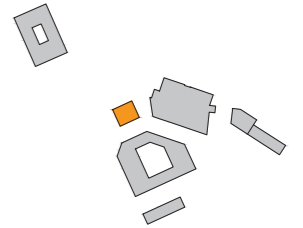
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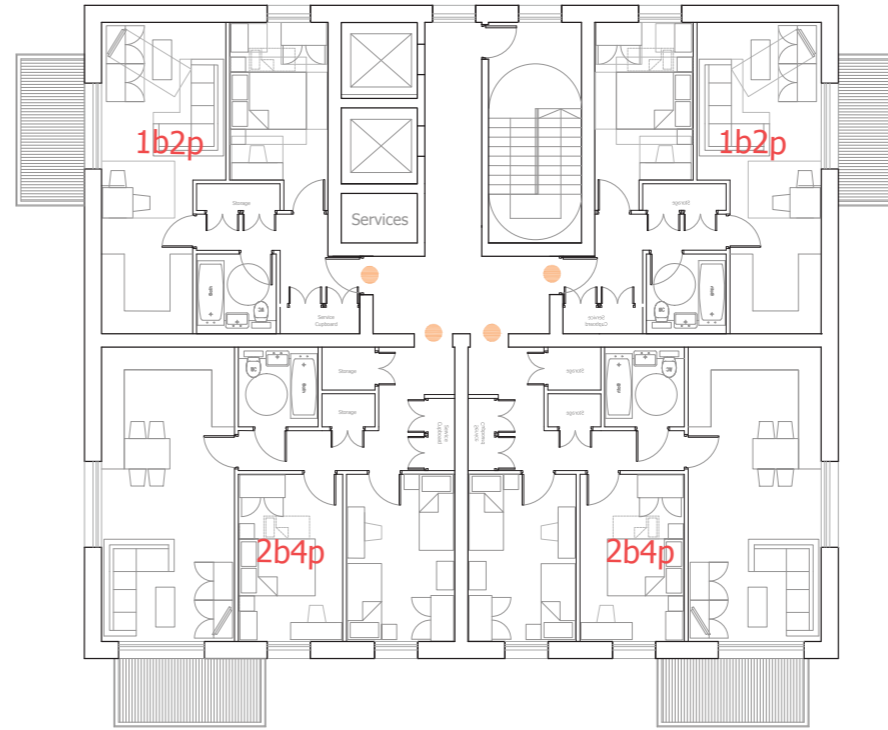


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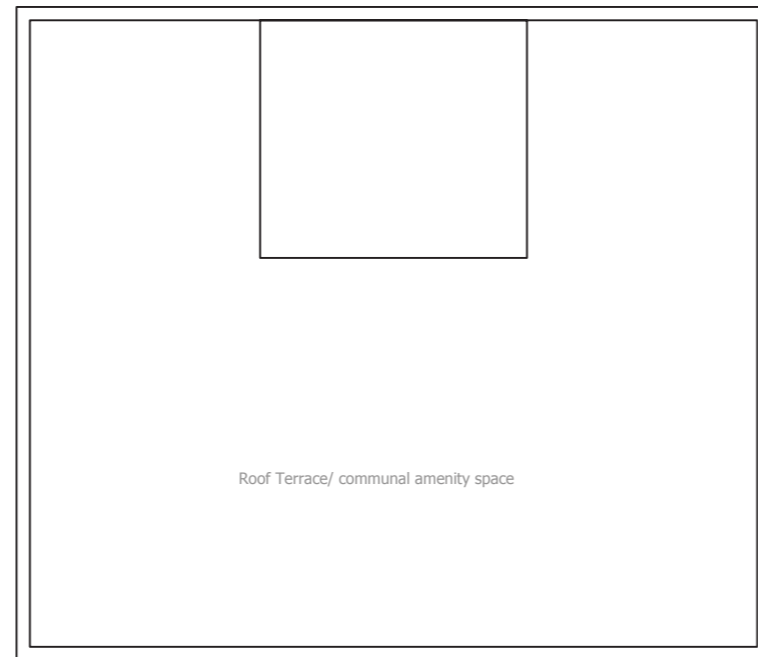
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Ground Floor



1st- 8th Floor Plan
(Typical Floor)



Roof Plan

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Project
**The Chocolate Factory
 Wood Green**

Drawing Title
**Block F Plans
 Ground, Typical and Roof Plan**

Date	Scale	Drawn by	Check by
09/11/15	1:100@A1	RA	MK
Project No	Drawing No	Revision	
25021	SK BF 100	-	



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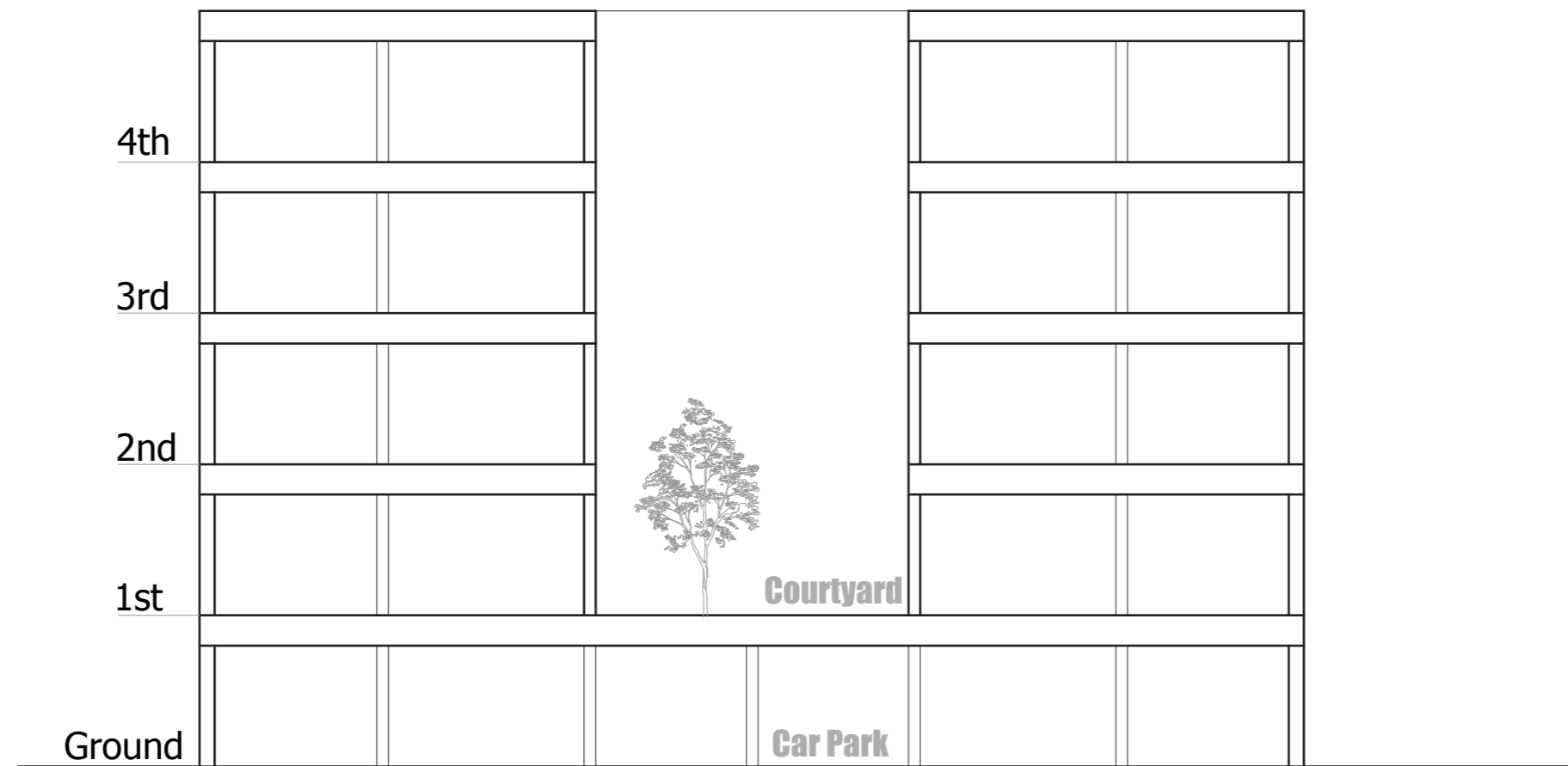
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OPTION B

- Ground Floor Car Park
- Podium
- 35 no parking spaces including 4 no disabled parking spaces
- 1300 sqm GIA per floor
- Atrium / Courtyard: roof Covering TBC



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Project
 The Chocolate Factory
 Wood Green

Drawing Title
 Block I
 Section

Date 06.10.15	Scale 1:200@A3	Drawn by RA	Check by MK
Project No 25031	Drawing No SK BI 250	Revision -	



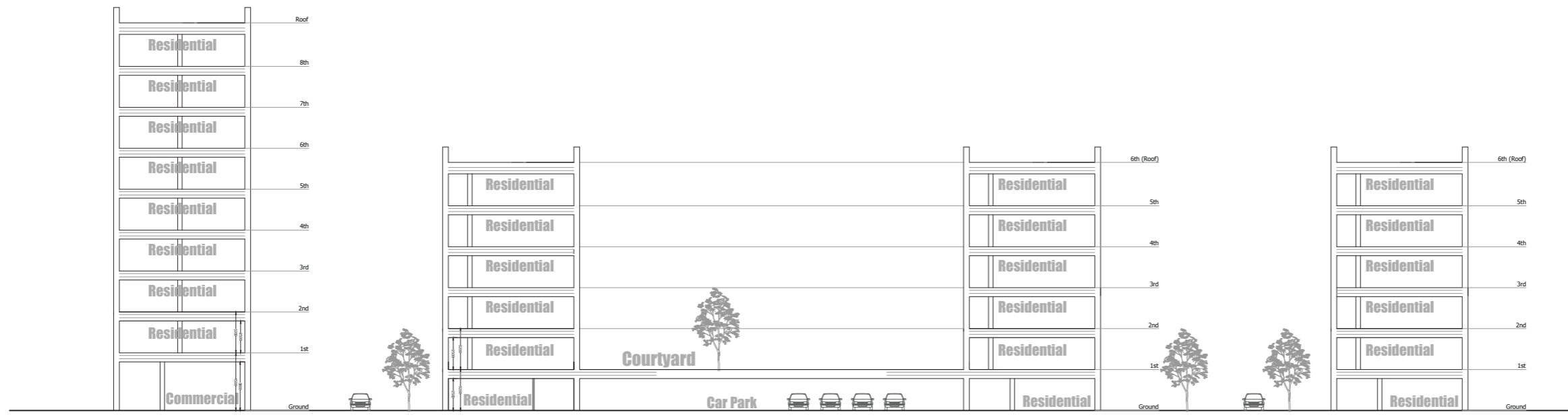
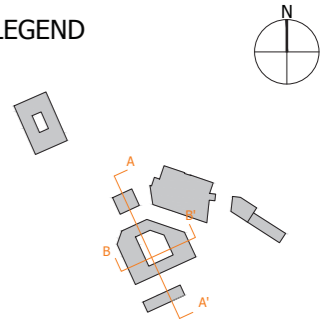
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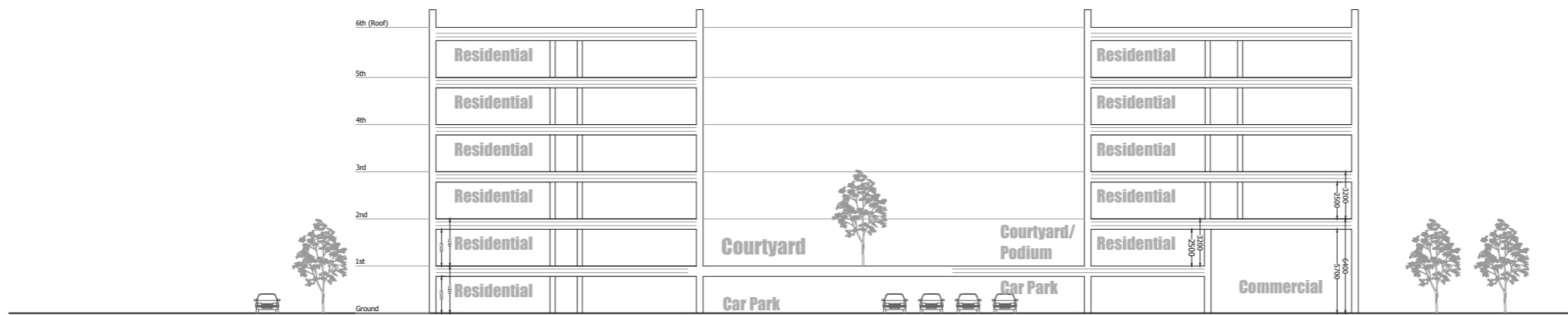


BLOCK F

BLOCK E

BLOCK D1

SECTION A - A'



BLOCK E

SECTION B - B'

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Project
 The Chocolate Factory
 Wood Green

Drawing Title
 Sections
 Site Plan Sections

Date	Scale	Drawn by	Check by
23.09.2015	1:200@A1	MK	MK
Project No	Drawing No	Revision	
25021	SK250	-	



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The background of the slide is a blurred image of traffic cones. On the left, a large green cone is partially visible. The rest of the background is filled with a variety of other traffic cones in shades of orange, red, and yellow, all out of focus.

13 HIGHWAYS LAYOUT PLAN AND SUMMARY NOTE

To London Borough of Haringey, Transport for London
 From Steer Davies Gleave
 Date 26 November 2015
 Project The Chocolate Factory, Wood Green

Project No. 22866701

The Chocolate Factory - Transport Scoping Note

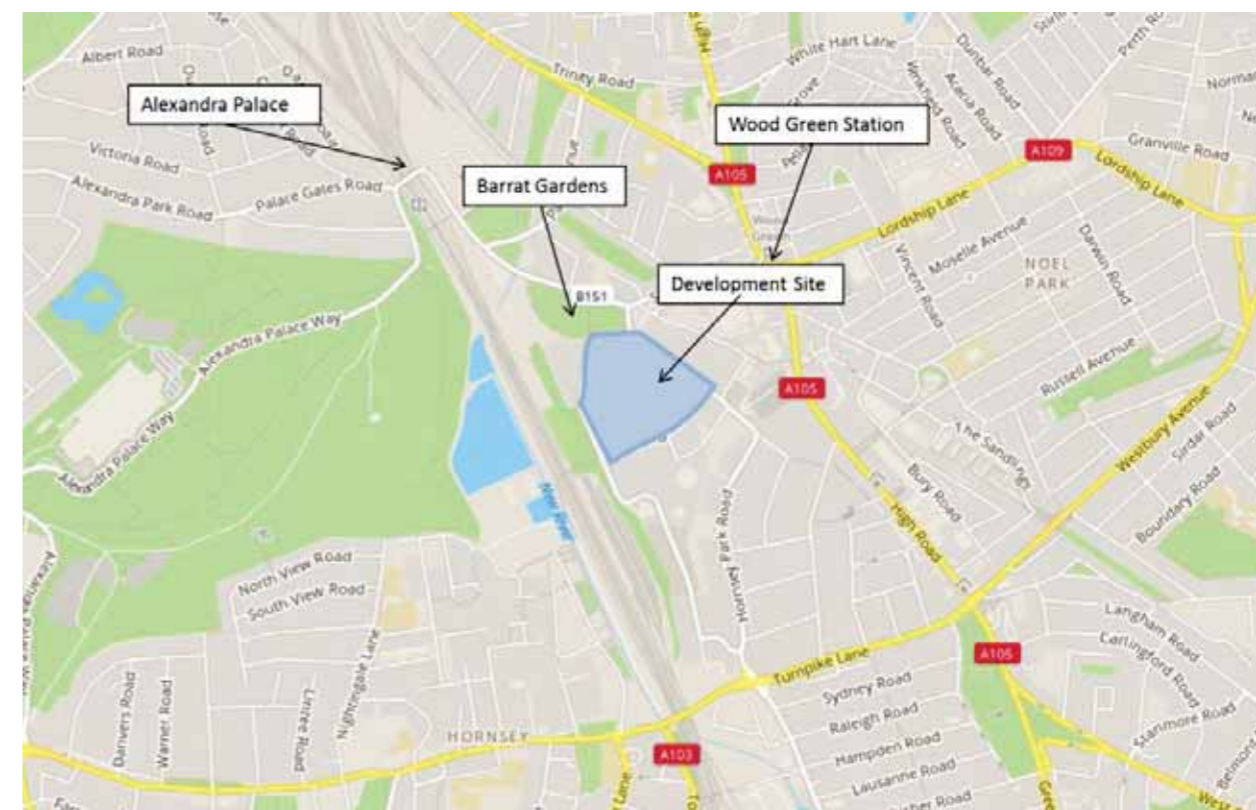
Introduction

1. This note provides details of the proposed scope of the Transport Assessment (TA) and associated supporting documents to be submitted as part of a future planning application for the redevelopment of The Chocolate Factory site in Wood Green (N22 6XJ), in the London Borough of Haringey (LBH).
2. LBH is the local planning authority. The scheme will be referable to the Greater London Authority and Transport for London (TfL) will be a key stakeholder. It is proposed to undertake formal pre-application consultation with LBH and TfL on the proposed development and scope of the planning application transport documents.
3. It is proposed to submit the following transport documents with the planning application:
 - TA including framework Construction Logistics Plan (CLP) and Delivery and Servicing Plan (DSP); and
 - Full Residential Travel Plan and Framework Workplace Travel Plan.
4. The TA for the redevelopment of The Chocolate Factory will be produced in accordance with TfL's latest Guidance. The Travel Plan(s) prepared will also follow the latest TfL Travel Planning Guidance and will be assessed with ATTrBuTE to determine their quality and robustness. A Transport chapter will also be provided as part of the Environmental Statement.

Baseline Transport Conditions

5. The development site occupies an area of land bounded by Mayes Road, Western Road and Coburg Road, 500 metres (6 minutes' walk) south west of Wood Green Underground station. The site is located adjacent to the East Coast Main Line, with National Rail services operating between Kings Cross station and the North of England. The eastern edge of the development site is bound by residential buildings, and the south, opposite Coburg Road, is occupied by commercial spaces. Barratt Gardens is situated 100 metres north of the site development. Clarendon Road intersects the site from Coburg Road and provides vehicular access to the centre of the site.
6. The development site is currently occupied by office buildings and commercial spaces leased by Workspace Group.
7. The site is located 500m (6 minutes' walk) south east of Alexandra Palace Station, and 1,200m (15 minutes' walk) north of Hornsey Station.
8. The location of the site is shown overleaf in Figure 1.

Figure 1: Site Location Map



Local Highway Network

9. The development site is well connected to the local and regional road network.
10. The B151 Mayes Road, which runs adjacent to the east of the development, meets the east-west-running B151 Station Road north of the site. The B151 Station Road links to the north-south A105 High Road which extends from Canonbury, between Highbury and Dalston towards Enfield Town.
11. Western Road, adjacent to the western site boundary, is an extension of Mary Neuner Road when accessed from a southern direction. It can be accessed from the north from the B151 Mayes Road roundabout.
12. Coburg Road is situated south of the development, and is a connecting road between Western Road and Mayes Road. Silsoe Road provides access from the south.
13. Clarendon Road intersects the development site from the south, running north from a junction with Coburg Road.

Public Transport Accessibility Level (PTAL)

14. A 'Public Transport Accessibility Level' (PTAL) assessment has been undertaken for the site. PTAL is a detailed measure of the accessibility of a location to the public transport network, taking into account walk access time and service availability.
15. The development site has a predominant PTAL of 4/5, demonstrating 'Good to Very Good' access to the public transport network. However, some portions of the wider site have a lower PTAL of 3.

Local Bus Services

16. There are a total of 12 frequent daytime bus services which operate from Wood Green Station within 500m of the site, facilitating connections with destinations in north and central London. The closest bus stops to the development site (H/K) are located on Station Road, 150m (2 minutes' walk) north of the site. Table 1 shows a summary of the services operating in the vicinity in terms of routes and frequency.

Table 1: Local Bus Services

Service	Route	Frequency (buses per hour)
29	Wood Green – Trafalgar Square	12
67	Aldgate East – Wood Green	6
121	Enfield Lock – Wood Green	6
123	Ilford – Wood Green	5
141	London Bridge – Wood Green – Palmers Green	9
184	Barnet – Wood Green – Turnpike Lane	7
221	Edgware – Wood Green – Turnpike Lane	10
232	St. Raphael's – Wood Green	4
243	Waterloo – Wood Green	9
329	Enfield Town – Wood Green	9
W3	Finsbury Park – Wood Green – Northumberland Park	6
W4	Ferry Lane Estate – Wood Green – Oakthorpe Park	6

London Underground

17. Wood Green is the closest London Underground station, situated approximately 430m (6 minutes' walk) north east of the site. The station is in Oyster Zone 3 and is served by the Piccadilly line. The Piccadilly line provides direct train services to Kings Cross St. Pancras, Holborn, Leicester Square, Green Park, South Kensington and destinations in south west London.

National Rail

18. Alexandra Palace Station is the nearest mainline rail station, approximately 490m (6 minutes' walk) north of the site. The station is served by Great Northern Rail services, providing connections to London and Welwyn Garden City, Hertford and Stevenage. Key destinations to the south include Old Street, Moorgate, Highbury and Islington and London Kings Cross. Five trains per hour run to London Kings Cross during the AM peak hour (08.00-09.00) with an approximate journey time of 21 minutes.
19. Hornsey Station is located 1,200m (15 minutes' walk) south west of the site. Also, served by Great Northern Rail services, it provides connections to Moorgate and London Kings Cross. During the AM peak hour, 4 trains run per hour to Moorgate with a journey time of 18 minutes.

Car Club

20. Car club provider Zipcar provides access to cars parked at a variety of locations across London. The closest Zipcar vehicle to the site is located on Parkland Road, approximately 250m (3 minutes' walk) east of the site. Cars are also located on High Road, 500m (6 minutes' walk) north east of the site.
21. City Car Club similarly provides car access across the London. The closest City Car Club vehicle to the site is located on Chadwell Lane, 550m (6 minutes' walk) south west of the site.

Pedestrian Access

22. The site has good pedestrian accessibility to surrounding retail, employment, leisure and public transport nodes. The site is located in reasonable distance to The Mall at Wood Green, 400m (5 minutes' walk) to the south east of the site. National Rail and London Underground services are located less than a kilometre away, in achievable walking distances from the site.
23. Wood Green town centre has recently benefitted from a public realm and highways improvement scheme which has improved pedestrian amenity in the local area. Features include widened pavements, seating and refuge areas, removal of guard railings and the introduction of Legible London wayfinding.
24. Footways in the vicinity of the development site are well maintained with street lighting located at regular intervals, dropped kerbs at junctions and pedestrian zebra crossings connecting the north of the site to the local footway network. Pedestrian access to the site is from Mayes Road, Western Road, Coburg Road and Clarendon Road.

Cycle Access

25. There are currently no designated cycle routes within the vicinity of the site. No Santander Cycle Hire Docking Stations are local to the site, with the closest docking stations located in Camden and Angel.
26. The highway network in the vicinity can be however be used by cyclists with care. Cycle parking is provided on Wood Green High Road adjacent to Wood Green Station and close to The Mall shopping centre.

Policy Context

27. The TA will have regard to the National Planning Policy Framework (NPPF) (2012) and National Planning Practice Guidance (NPPG) (2014). The policies within the NPPF/NPPG convey the Government's vision of sustainable development, which should be interpreted and applied locally to meet local aspirations.
28. The London Plan (March 2015) is the overarching regional planning document applicable in relation to the development. In addition, particular reference will be made to the London Borough of Haringey (LBH) Local Development Framework and the guidance contained within, but not limited to:
- LB Haringey - Local Plan: Strategic Policies (2013);
 - LB Haringey - Saved Unitary Development Plan (UDP) (2013);
 - LB Haringey - Sustainable Design and Construction Supplementary Planning Document (SPD) (2013);
 - LB Haringey - Haringey Heartlands Development Framework (2005); and
 - LB Haringey - Draft Development Management Development Plan Document (DPD) (2015).
29. Consideration will also be given to relevant guidance documents relating to accessibility.

Development Proposals

30. The current development proposals comprise:
- Up to 11,500 sqm of commercial floorspace, to be predominantly occupied by Workspace Group;
 - Up to 300 dwellings, comprising a mix of 1, 2 and 3 bed units;
 - Up to 4,500 sqm of community floorspace;
 - New vehicular access points from Western Road and retained access from Coburg Road; and
 - Provision of high quality Public Open Space.

Car and Cycle Parking

31. Policy DM43 of the LBH Draft Development Management DPD (2015) states that ‘development proposals will be assessed against the car parking and cycle parking standards set out in the London Plan’.
32. Car parking will be provided in accordance with London Plan Standards. However, the proposed car parking ratio at the site is yet to be defined and will be developed following pre-application discussions. As a minimum, ten percent of residential units (circa 26-30 dwellings) will be provided with a dedicated disabled parking bay. Appropriate disabled provision will also be provided for the commercial/community elements of the scheme. Provision for electronic charging spaces will also be provided in accordance with the relevant standards set out in the London Plan.
33. Secure, integrated, convenient and accessible cycle parking facilities will be provided on site. Whilst the type of storage has yet to be defined, it is likely two-tier cycle racks will be incorporated into the design. Assuming the quantum of development set out within this note the site would require a total of approximately 430-515 residential and approximately 90 commercial cycle parking spaces. For the community floorspace circa 60 spaces will need to be provided, however this is subject to confirmation of expected employee numbers. Consideration will also be given to larger cycle storage areas to facilitate adapted bicycles for people with a disability.

Waste Generation & Storage

34. The Chocolate Factory development features multiple land use types (C3 Residential, B1 Commercial and D1 Community use) and therefore the waste and servicing strategy for each element of the site will differ according to its land use.
35. It is assumed that waste for the development will be stored in communal waste stores at street level and within 10 metres of a place suitable for a collection vehicle to stop. This would help to minimise space and facilitate collection on waste collection days.
36. 1100 litre Eurobins could be used for general waste and recyclables at the development. Based on this bin size, the waste storage requirements for the development are set out in Table 2 below.

Table 2: Chocolate Factory Indicative Waste Storage Provision

Land Use	Waste Stream	Storage Required (m ³)	1100L Eurobins Required	Days Storage Required
Residential	General Waste	37.4	34	7 days
	Recyclables	24.9	23	
Commercial	General Waste	8.0	8	
	Recyclables	8.0	8	

Trip Generation

37. The Trip Generation section of the TA will outline the existing trip generation of the site, the proposed trip generation and the net change in trips.
38. A survey of the existing use of the Site will be conducted in early 2016. The number of vehicles and pedestrians entering and exiting the site will be recorded across a neutral weekday. This data will be used as a basis to forecast the net impact of the development. A scoping note detailing the survey methodology at the existing site is provided at **Appendix A** at the rear of this note.

39. The latest TRICS database has been used to assess the likely trips to be generated by the proposed development. Similar TRICS sites for C3 Residential and B1 Commercial land uses have been used to derive trip rates for the Chocolate Factory development.
40. The proposed trip rates are presented in Table 3 and Table 4 below. They will be applied to the C3 (residential) and B1 (employment) elements of the development.

Table 3: Residential Trip Rates per Unit (C3)

Time	In	Out	Total
AM (0800-0900)	0.18	0.58	0.76
PM (1700-1800)	0.35	0.16	0.51

Table 4: Commercial Trip Rates per 100m² GFA (B1)

Time	In	Out	Total
AM (0800-0900)	2.04	0.18	2.22
PM (1700-1800)	0.31	2.53	2.84

41. The Community land use will be assessed in a similar manner once further details emerge through the design process.
42. Residential and commercial mode split percentages will be obtained from the latest available 2011 Census Method of Travel to Work Data and refined where necessary in accordance with the car parking ratio proposed on site and local levels of car ownership.

Delivery and Servicing Trips

43. Servicing and delivery trips associated with the proposed development will be added to the network separately. Steer Davies Gleave holds a substantial database of servicing and delivery information for a range of mixed use developments across London. The servicing database will be used to forecast the number of servicing vehicle trips associated with the proposed development.
44. The trip rates given below provide a robust basis for the assessment of servicing and delivery vehicles to be accommodated within the development:
 - C3 Residential (including supermarket deliveries) – Daily trip rate of 0.10 vehicles per 100m² NIA
 - B1 Employment – Daily trip rate of 0.21 vehicles per 100m² NIA
 - D1 Community – Daily trip rate of 0.15 vehicles per 100m² NIA
45. Construction vehicle movements will be identified in a CLP and delivery, waste and servicing movements will be highlighted in a DSP, both of which will be prepared as part of the overall TA.

Cumulative Schemes

46. The list of cumulative schemes to be taken into account within this assessment will be discussed and finalised with LBH. Schemes that have been granted planning permission or where applications have been submitted will be assessed. Trips that are generated by cumulative schemes will be taken directly from the relevant scheme TAs.

Impact Assessment

47. It is proposed to assess the following scenarios for the TA and EIA:
- Baseline (2016) = Existing Baseline.
 - Future Baseline without Development = Baseline + cumulative schemes.
 - Future Baseline with Development = Baseline + cumulative schemes + proposed development.
48. For the purposes of the TA a baseline year of 2016 will be established to provide a robust baseline scenario. Baseline traffic conditions will be established for key access roads, junctions and public transport nodes in the vicinity that may be affected by material changes as a result of the development.
49. Baseline public transport data will establish existing loadings on London Underground services at Wood Green Station using the latest origin-destination surveys. An overview of bus routes and stops in the vicinity will also be provided alongside Key Points data if available from TfL.
50. Pedestrian and cycle accessibility and flows will also be examined on key links surrounding the site. A PERS Audit will be conducted across key links and crossings.
51. A minimum of three years' road accident data for the local road network will be obtained from TfL and assessed.

Mitigation

52. Appropriate mitigation measures will be identified and considered, including the following:

Travel Planning

53. A Residential Travel Plan will be prepared focusing on site-specific measures for residential tenants, with a commitment to implementing the measures once the residential units are occupied. Essentially the travel plan will include:
- information about the development and the policy context;
 - accessibility assessment to and from the site and the existing travel options;
 - a description of the objectives and SMART targets of the plan;
 - a plan for the delivery of the Travel Plan objectives;
 - a catalogue of the Travel Plan measures and an Action Plan; and
 - plans and timescales for Travel Plan development, monitoring and review.
54. A Framework Workplace Travel Plan will be prepared to cover all commercial/community uses on site.

Delivery and Servicing Plan

55. As part of the planning application a Delivery and Servicing Plan (DSP) will be produced. A DSP provides a framework to better manage all types of freight vehicle movement to and from individual developments. The DSP will set out the estimated number of waste and servicing trips to and from development, the proposed servicing arrangements and the strategy devised in order to encourage sustainable freight movements. The DSP will be appended to the TA.

Construction Logistics Plan

56. A Construction Logistics Plan (CLP) will also be produced. A CLP provides a framework to better manage all types of freight vehicle movement to and from construction sites and is essentially the equivalent of a Workplace Travel Plan for construction vehicles. The CLP will set out the construction programme, the estimated number of construction vehicle trips, and the construction strategy devised in order to encourage sustainable freight movements. The CLP will also be appended to the TA.

Appendix A – Traffic Survey Scoping Note

To Traffic Survey Company
 From Steer Davies Gleave
 Date 20 November 2015
 Project The Chocolate Factory, Wood Green

Project No. 22866701

Traffic Survey Scope

- This note sets out the required traffic surveys on the highway surrounding the Chocolate Factory site in Wood Green, Haringey.

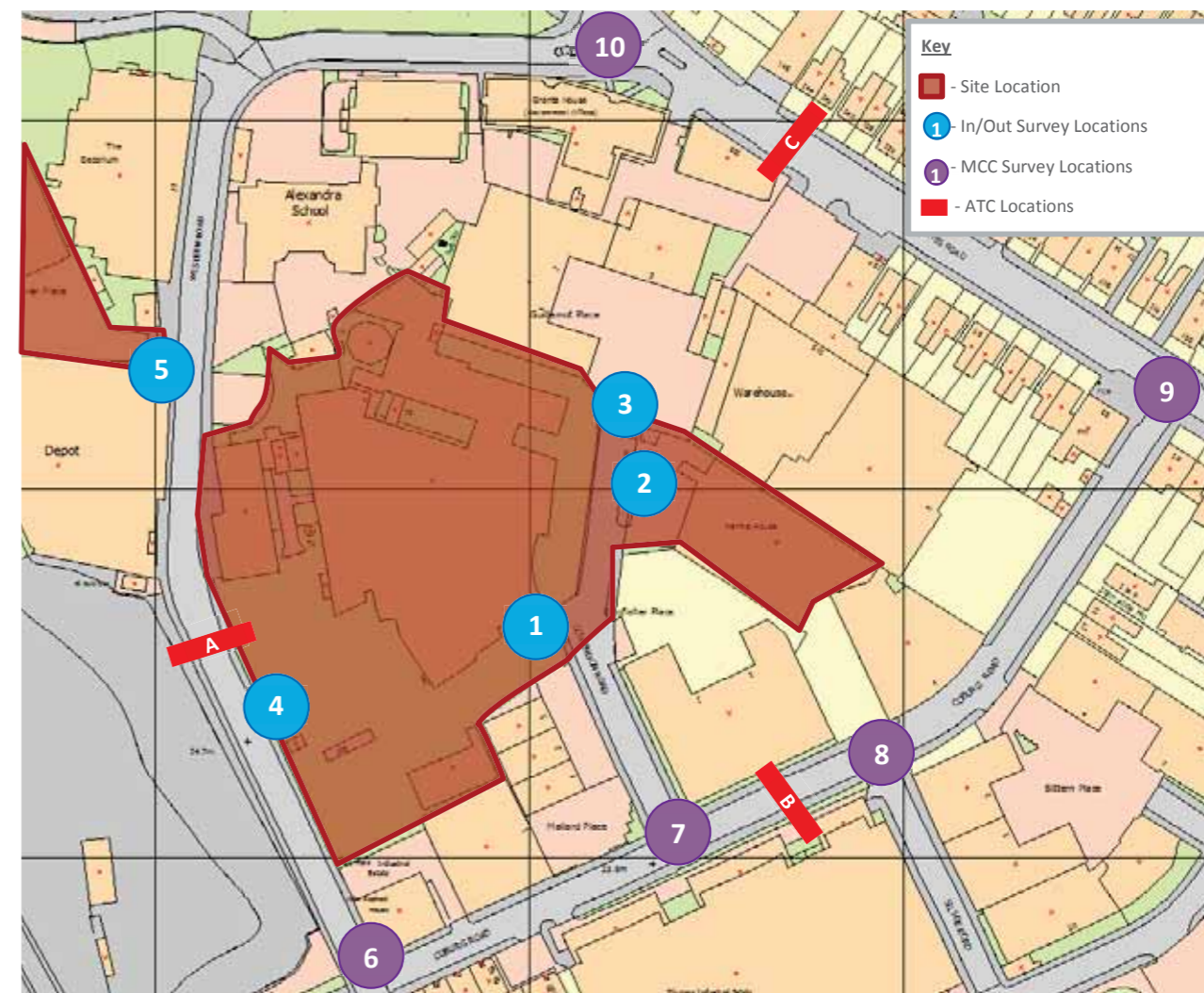
Key Survey Requirements

- Multi-directional Turning Count Survey conducted on a neutral weekday (Tuesday, Wednesday or Thursday)
 - All In/Out surveys conducted on one day between the hours of 06:00-20:00. Can you also provide a separate fee quote for undertaking peak hour counts from 07:00 – 10:00 and 16:00-19:00.
 - All MCC surveys conducted on one day between the hours of 07:00 – 10:00 and 16:00-19:00.
 - All ATC surveys conducted over a 7 day period, 24 hours a day.
 - All vehicles recorded (in 15 minute intervals) at all relevant junctions and links during the day (see diagram overleaf for locations).
 - Vehicles to be classified in line with standard TfL classification.
 - Pedestrian counts at the locations identified.
 - Survey company to obtain relevant approvals for cameras etc. from Local Authority (London Borough of Haringey).
 - Survey company to report any unusual incidents.
 - Excel based tabulated results provided within 7 days of completion alongside DVD or electronic copy of video surveys.
- Could you please respond with a quote based on the above to Simon.Edwards@sdgworld.net before close of play on Wednesday 25th November 2015.

Site Location

- The location of the required survey points are shown on Figure 1 on the following page.

Figure 1: Site and Survey Locations



Count Requirements

In/Out Car Park Counts

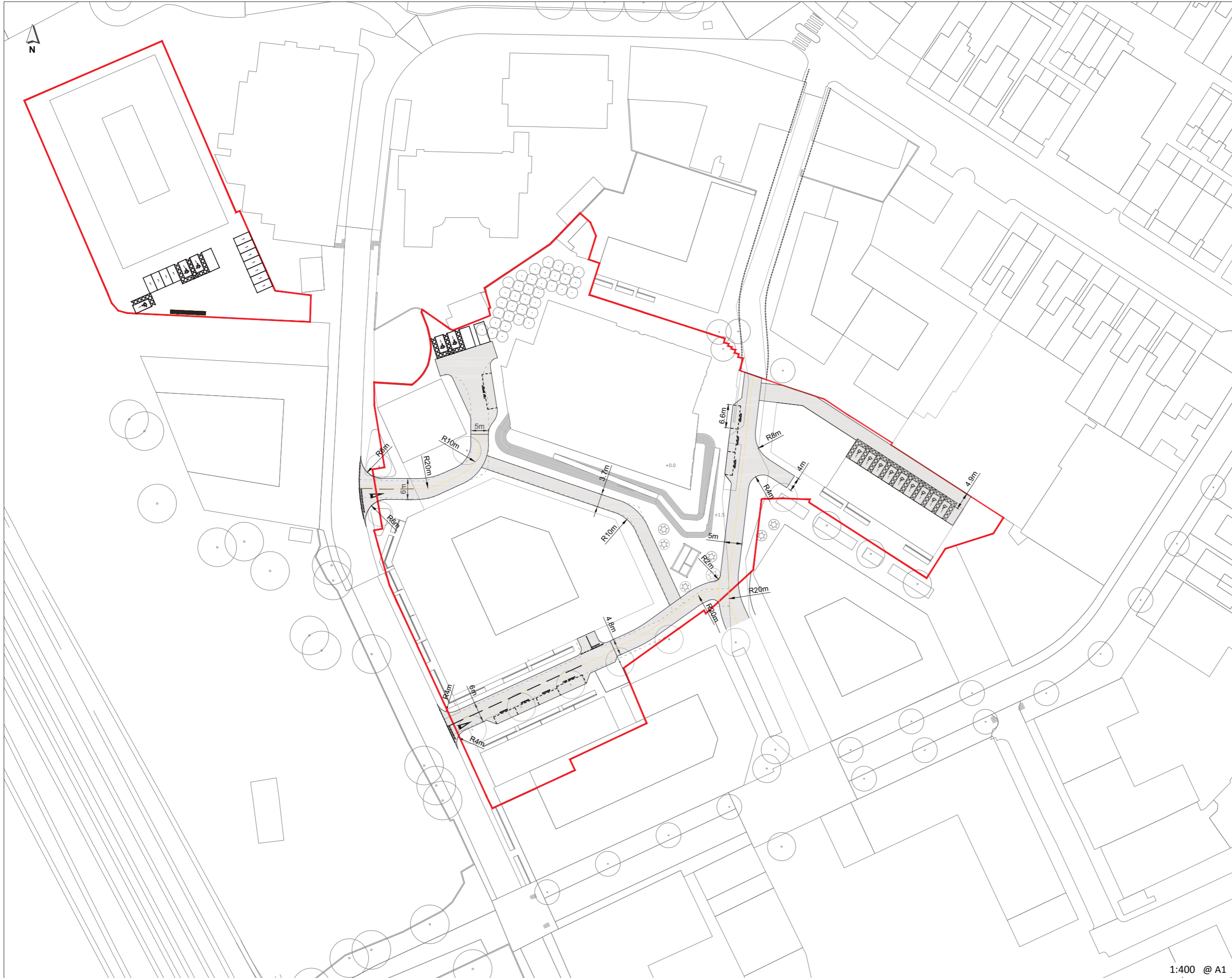
- Location 1:** The Chocolate Factory Car Park Access onto Clarendon Road (as shown in Figure 2)
 - Requirement 1:** In and out counts for all vehicles recorded in 15 minute intervals in line with standard TfL classification.
 - Requirement 2:** Full pedestrian in and out counts.

Figure 2: Chocolate Factory Car Park Access



- **Location 2:** Parma House Car Park Access onto Clarendon Road
 - **Requirement 1:** In and out counts for all vehicles from both entrance exit locations recorded in 15 minute intervals in line with standard TfL classification.
 - **Requirement 2:** Full pedestrian in and out counts.
- **Location 3:** Guillemot Car Park Access onto Clarendon Road
 - **Requirement 1:** In and out counts for all vehicles recorded in 15 minute intervals in line with standard TfL classification.
 - **Requirement 2:** Full pedestrian in and out counts.
- **Location 4:** The Chocolate Factory Car Park Access onto Western Road
 - **Requirement 1:** In and out counts for all vehicles recorded in 15 minute intervals in line with standard TfL classification.
 - **Requirement 2:** Full pedestrian in and out counts.
- **Location 5:** Metropolitan Police Car Park Access onto Western Road
 - **Requirement 1:** In and out counts for all vehicles recorded in 15 minute intervals in line with standard TfL classification.
 - **Requirement 2:** Full pedestrian in and out counts.
- **Location 6:** Manual Classified Count (MCC) Survey at Coburg Road / Western Road Priority Junction
 - **Requirement 1:** Multi-modal all direction turning counts recorded in 15 minute intervals in line with standard TfL classification.

- **Location 7:** MCC Survey at Clarendon Road / Coburg Road Priority Junction
 - **Requirement:** Multi-modal all direction turning counts recorded in 15 minute intervals in line with standard TfL classification
- **Location 8:** MCC Survey at Coburg Road / Silsoe Road Priority Junction
 - **Requirement:** Multi-modal all direction turning counts recorded in 15 minute intervals in line with standard TfL classification
- **Location 9:** MCC Survey at Coburg Road / Mayes Road Priority Junction
 - **Requirement:** Multi-modal all direction turning counts recorded in 15 minute intervals in line with standard TfL classification
- **Location 10:** MCC Survey at Mayes Road / Western Road Roundabout
 - **Requirement:** Multi-modal all direction turning counts recorded in 15 minute intervals in line with standard TfL classification
- **Location A:** Automatic Traffic Count (ATC) Survey on Western Road
 - **Requirement:** Multi-modal automatic counts recorded in hourly intervals in line with standard TfL classification.
- **Location B:** ATC Survey on Coburg Road
 - **Requirement:** Multi-modal automatic counts recorded in hourly intervals in line with standard TfL classification.
- **Location C:** ATC Survey on Mayes Road
 - **Requirement:** Multi-modal automatic counts recorded in hourly intervals in line with standard TfL classification.



Rev.	Date	Purpose of sketch	Des	Chk	App
-	18/12/15	FOR INFORMATION	JZC	SJE	

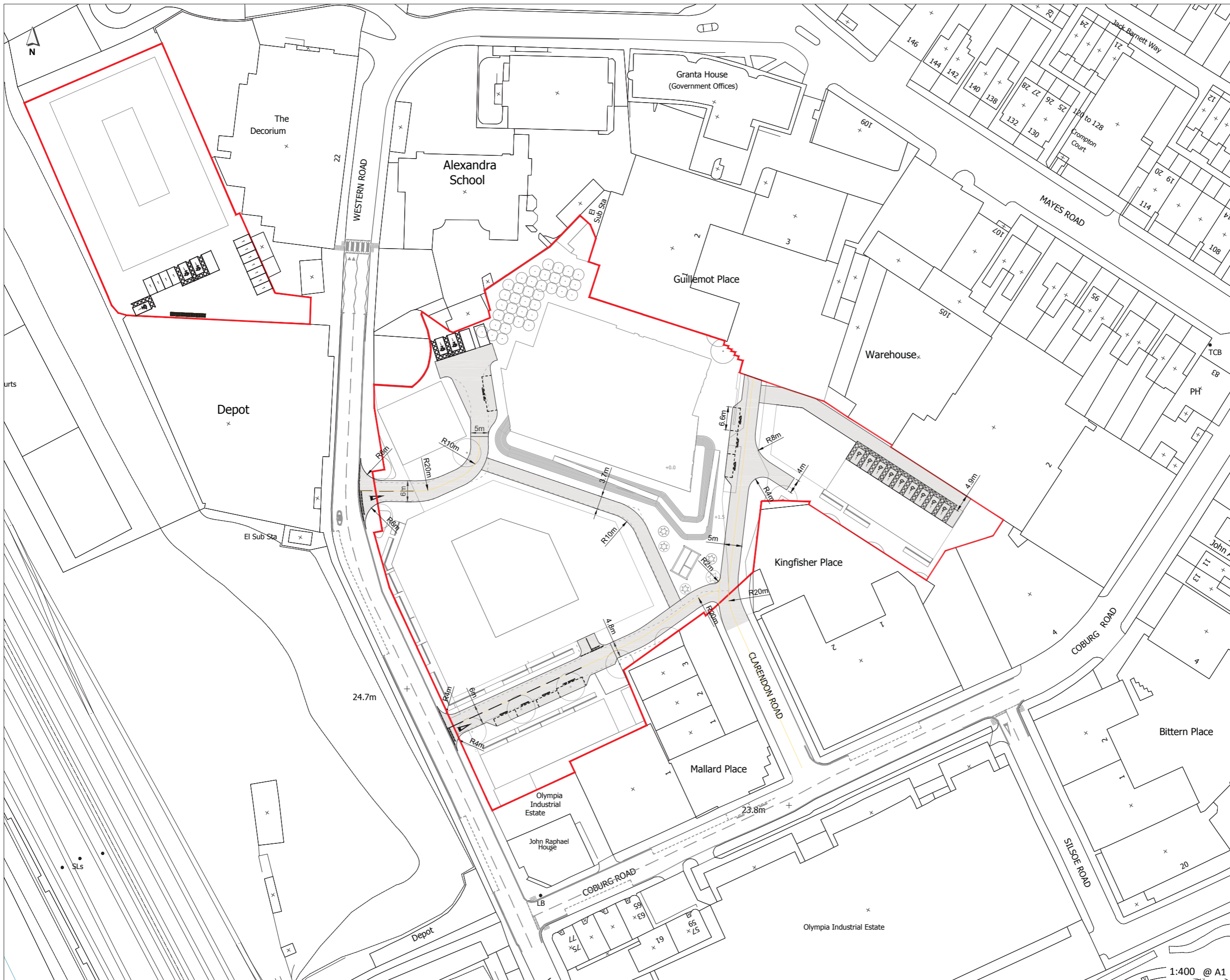
steer davis gleave
 t +44 (0)20 7910 5000 e sdginfo@sdgworld.net

Sketch Title:
**THE CHOCOLATE FACTORY
 PROPOSED HIGHWAYS LAYOUT
 ILLUSTRATIVE MASTERPLAN**

Sketch No. 20716-SDG-SK-DRG-TRA-003	Sheet No. 01 of 01
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WORK IN PROGRESS

Rev.	Date	Purpose of sketch	Des	Chk	App
-	18/12/15	FOR INFORMATION	JZC	SJE	

steer davies gleave
 t +44 (0)20 7910 5000 e sdginfo@sdgworld.net

Sketch Title:
**THE CHOCOLATE FACTORY
 PROPOSED HIGHWAYS LAYOUT
 DETAILED APPLICATION**

Sketch No.
 20716-SDG-SK-DRG-TRA-001

Sheet No.
 01 of 01

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 DATE: 18/12/2015



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